

STATE OF OHIO
DEPARTMENT OF TRANSPORTATION

MOT-BP-FY18

CITY OF DAYTON

MIAMI TOWNSHIP

WASHINGTON TOWNSHIP

MONTGOMERY COUNTY

SEE SHEET 2 FOR LOCATION MAP

LOCATION MAP

DESIGN DESIGNATION
SEE SHEET 2 FOR ADT

DESIGN EXCEPTIONS
NONE

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PROJECT DESCRIPTION

PAINT EXISTING STEEL SUPERSTRUCTURES AND SEAL
CONCRETE SURFACES AS DETAILED IN THE PLANS.

PROJECT EARTH DISTURBED AREA: N/A ACRES *
ESTIMATED CONTRACTOR EARTH DISTURBED AREA: N/A ACRES *
NOTICE OF INTENT EARTH DISTURBED AREA: N/A ACRES *
*MAINTENANCE PROJECT

LIMITED ACCESS

THIS IMPROVEMENT IS ESPECIALLY DESIGNED FOR
THROUGH TRAFFIC AND HAS BEEN DECLARED A LIMITED
ACCESS HIGHWAY OR FREEWAY BY ACTION OF THE
DIRECTOR IN ACCORDANCE WITH THE PROVISIONS OF
SECTION 5511.02 OF THE OHIO REVISED CODE.

2016 SPECIFICATIONS

THE STANDARD SPECIFICATIONS OF THE STATE OF
OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING
CHANGES LISTED IN THE PROPOSAL SHALL GOVERN
THIS IMPROVEMENT.

I HEREBY APPROVE THESE PLANS AND DECLARE THAT
THE MAKING OF THIS IMPROVEMENT WILL NOT REQUIRE
THE CLOSING TO TRAFFIC OF THE HIGHWAY EXCEPT AS
DESCRIBED ON SHEET 4-5 AND THAT PROVISIONS FOR
THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE
AS SET FORTH ON THE PLANS AND ESTIMATES.

RAILROAD INVOLVEMENT

NONE

CONSTRUCTION PROJECT NO.


102303

FEDERAL PROJECT NO.

E160(258)

MOT-BP-FY18

1
71

| | |
|---|---------------------------------------|
| UNDERGROUND UTILITIES | |
| CONTACT BOTH SERVICES TWO WORKING DAYS BEFORE YOU DIG. | |
|  OHIO Utilities Protection SERVICE (Non-members must be called directly) | Call Before You Dig 1-800-362-2764 |
| OIL & GAS PRODUCERS UNDERGROUND PROTECTION SERVICE 1-800-925-0988 | |

PLAN PREPARED BY:
OHIO DEPARTMENT OF TRANSPORTATION
DISTRICT 7
1001 ST MARYS AVENUE
SIDNEY, OHIO

ENGINEERS SEAL:



SIGNED: *Chris Weiss*
DATE: 2/10/17

| STANDARD CONSTRUCTION DRAWINGS | | | | | | SUPPLEMENTAL SPECIFICATIONS | SPECIAL PROVISIONS |
|--------------------------------|---------|-----------|----------|--|--|--------------------------------|-----------------------|
| MT-95.30 | 7/21/17 | TC-41.20 | 10/18/13 | | | 800-2016 10/20/17 | |
| MT-95.31 | 7/21/17 | TC-42.20 | 10/18/13 | | | 821 4/20/12 | |
| MT-95.40 | 1/20/17 | TC-52.10 | 10/18/13 | | | 832 1/17/14 | |
| MT-95.41 | 7/21/17 | TC-52.20 | 7/21/17 | | | 321 4/20/12 | |
| MT-95.45 | 7/21/17 | | | | | | |
| MT-95.50 | 7/21/17 | TC-71.10 | 1/19/18 | | | | |
| MT-98.28 | 1/20/17 | TC-72.20 | 7/15/16 | | | | |
| MT-98.29 | 1/20/17 | | | | | | |
| MT-98.30 | 7/21/17 | | | | | | |
| MT-99.30 | 7/21/17 | MT-99.20 | 7/21/17 | | | | |
| MT-101.70 | 1/17/14 | MT-101.60 | 1/20/17 | | | | |
| MT-101.75 | 7/15/16 | | | | | | |
| MT-102.10 | 1/20/17 | | | | | | |
| MT-102.20 | 7/18/14 | | | | | | |
| MT-105.10 | 7/19/13 | | | | | | |
| MT-110.10 | 7/19/13 | | | | | | |

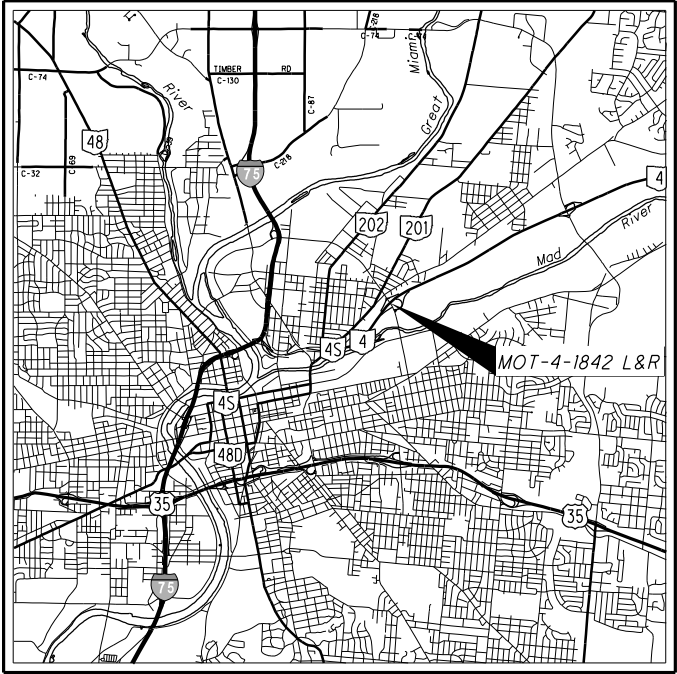
APPROVED: *Randy Chevalley* PE PS/SSUT
DATE: 2/10/17 DISTRICT DEPUTY DIRECTOR

APPROVED: *Kevin Whaley*
DATE: 1/27-17 DIRECTOR DEPARTMENT OF
TRANSPORTATION

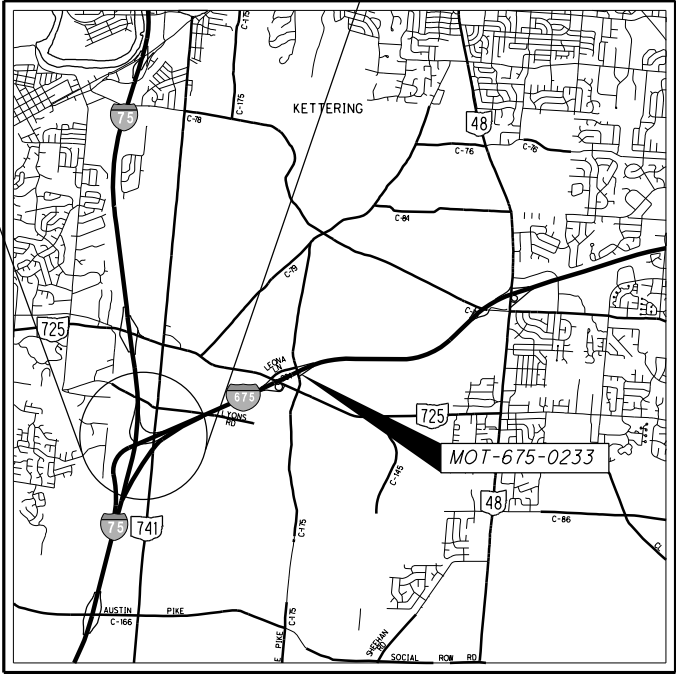
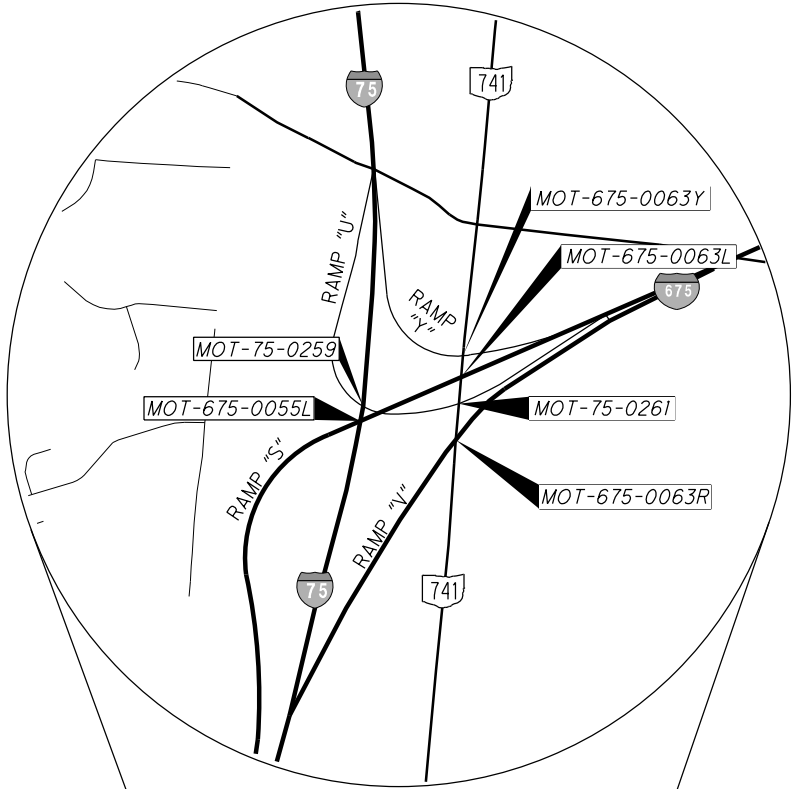
MOT - BP FY18
180145 PID - 102303
Dist 7 2/15/2018

Contract Proposal Available @
www.contracts.dot.state.oh.us/home
Conformed Set

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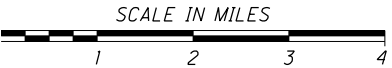


LOCATION MAP
SFN: 5700787 (L) 5700817 (R)
LATITUDE: 39°46'43" LONGITUDE: 84°9'43"



MOT-75-0259
SFN: 5706432
LATITUDE: 39°37'22" LONGITUDE: 84°13'46"
MOT-75-0261
SFN: 5710782
LATITUDE: 39°37' 22" LONGITUDE: 84°13'37"
MOT-675-0055
SFN: 5710707 (L)
LATITUDE: 39°37'21" LONGITUDE: 84°13'51"
MOT-675-0063
SFN: 5710766 (L)
LATITUDE: 39°37'27" LONGITUDE: 84°13'37"

MOT-675-0063Y
SFN: 5710731
LATITUDE: 39°37'28" LONGITUDE: 84°13'36"
MOT-675-0063R
SFN: 5710804
LATITUDE: 39°37' 19" LONGITUDE: 84°13'37"
MOT-675-0233
SFN: 5711126
LATITUDE: 39°38'01" LONGITUDE: 84°11'55"



PORTION TO BE IMPROVED
INTERSTATE HIGHWAY
FEDERAL ROUTES
STATE ROUTES
COUNTY & TOWNSHIP ROADS
OTHER ROADS

| ADT | |
|---|-------------------------|
| MOT-4-18.42L (SR-4 OVER STANLEY AVE.): MOT-4-18.42R (SR-4 OVER STANLEY AVE.): | ADT ON STANLEY = 15,000 |
| MOT-75-02.59 (IR-675 RAMP "U" OVER IR-75): | ADT ON IR-75 = 81,550 |
| MOT-75-02.61 (IR-675 RAMP "U" OVER SR-741): | ADT ON SR-741 = 24,700 |
| MOT-675-00.55L (IR-675 RAMP "S" OVER IR-75): | ADT ON IR-75 = 81,550 |
| MOT-675-00.63Y (IR-675 RAMP "Y" OVER SR-741): | ADT ON SR-741 = 24,700 |
| MOT-675-00.63L (IR-675 OVER SR-741): | ADT ON SR-741 = 24,700 |
| MOT-675-00.63R (IR-675 RAMP "V" OVER SR-741): | ADT ON SR-741 = 24,700 |
| MOT-675-02.33 (YANKEE STREET OVER IR-675): | ADT ON IR-675 = 70,250 |
| | |

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UTILITIES

THERE ARE NO UNDERGROUND UTILITIES SHOWN ON THIS PLAN. THE NATURE OF THE WORK REQUIRED BY THIS PROJECT WILL NOT AFFECT ANY KNOWN UNDERGROUND UTILITIES THAT EXIST UNDER OR ADJACENT TO THE WORK AREA.

MOT-4-18.42L/R - HIGHWAY LIGHTING

MIAMI VALLEY LIGHTING
1065 WOODMAN DRIVE
DAYTON, OH 45401
937-259-7780
BEN JONES

MOT-675-02.33 - HIGHWAY LIGHTING

ODOT DISTRICT 7
1001 ST MARYS AVE
SIDNEY, OHIO 45365
937-497-6897
JUSTIN YOH, TRAFFIC ENGINEER

MOT-75-0261, MOT-675-0063L, R AND Y
(NOTIFY THE PROJECT ENGINEER 14 DAYS PRIOR TO PAINTING.)

CHARTER CABLE
3691 TURNER RD
DAYTON, OHIO 45415
937-425-8850
TIM KUSS

CINCINNATI BELL (UNDERGROUND)
221 EAST FOURTH ST. (BUILDING 121-900)
CINCINNATI, OHIO 45201
513-565-6024
MIKE WILLIAMS

FIBERTECH NETWORKS LLC
585-743-1730
RICK WEISS

DAYTON POWER & LIGHT CO. (AERIAL)
1900 DRYDEN RD.
DAYTON, OHIO 45349
937-331-4521
WILLIAM GOURLEY

EXISTING STRUCTURE VERIFICATION

DETAILS AND DIMENSIONS SHOWN ON THESE PLANS PERTAINING TO THE EXISTING STRUCTURE HAVE BEEN OBTAINED FROM PLANS OF THE EXISTING STRUCTURE AND FROM FIELD OBSERVATIONS AND MEASUREMENTS. CONSEQUENTLY, THEY ARE INDICATIVE OF THE EXISTING STRUCTURE AND THE PROPOSED WORK, BUT THEY SHALL BE CONSIDERED TENTATIVE AND APPROXIMATE. THE CONTRACTOR IS REFFERED TO CMS SECTIONS 102.05 AND 105.02.

EXISTING BRIDGE PLANS

EXISTING BRIDGE PLANS MAY BE INSPECTED IN THE OFFICE OF STRUCTURAL ENGINEERING IN COLUMBUS, OHIO OR IN THE DISTRICT 7 OFFICE IN SIDNEY, OHIO. DURING NORMAL BUSINESS HOURS.

CLEARING AND GRUBBING

ALTHOUGH THERE ARE NO TREES OR STUMPS SPECIFICALLY MARKED FOR REMOVAL WITHIN THE LIMITS OF THE PROJECT, A LUMP SUM QUANTITY IS INCLUDED IN THE GENERAL SUMMARY FOR ITEM 201, CLEARING AND GRUBBING. ALL PROVISIONS AS SET FORTH IN THE SPECIFICATIONS UNDER THIS ITEM ARE INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 201, CLEARING AND GRUBBING.

ITEM 630 SIGNING, MISC.: BRIDGE IDENTIFICATION SIGN

THIS ITEM SHALL BE USED TO REMOVE AND RE-INSTALL THE EXISTING STRUCTURE IDENTIFICATION SIGN. CARE SHALL BE TAKEN DURING REMOVAL AS TO NOT DAMAGE THE EXISTING SIGN. IF THE SIGN IS DAMAGED DURING REMOVAL, THE CONTRACTOR SHALL PROVIDE A NEW SIGN AT NO COST TO THE DEPARTMENT. THE SIGN SHALL BE 24"X4". IT SHALL BE ALUMINUM WITH NON-REFLECTIVE WHITE SHEETING BACKGROUND AS PER CMS 730.20. LETTERS SHALL BE BLACK 2" HEIGHT, SERIES C STROKE WIDTH, AND SILK SCREENED AS PER CMS 730.22.

THE SIGN SHALL BE MOUNTED ON THE TOP OF THE CONCRETE BARRIER IN FRONT OF THE EXISTING SIGN LOCATION. SIGNS SHALL BE MOUNTED USING TWO 5/16" X 2-1/4" HEAVY DUTY CONCRETE SCREW ANCHORS.

ALL LABOR, MATERIAL, EQUIPMENT AND INCIDENTALS TO PERFORM THE WORK ABOVE SHALL BE INCLUDED IN THE UNIT PRICE BID FOR SIGNING, MISC.: BRIDGE IDENTIFICATION SIGN (EACH).

FIELD PAINTING STRUCTURAL STEEL, FINISH COAT

THE FINISH COAT FOR THE STRUCTURAL STEEL SHALL BE PAINTED BLUE (FEDERAL PAINT CODE 25230) FOR MOT-4-1842 L/R.

ALL OTHER STRUCTURES SHALL BE PAINTED BLUE (FEDERAL PAINT CODE 15526).

| BRIDGE | MOT Stage | 614 | 614 | | 614 | 614 | 614 | | 618 | 618 | | 622 | | 646 | 646 | |
|----------------|-----------|--|-----------|--|---|--|--|--|---|-----------------------------------|--|-----------------------|--|---------------|---------------|--|
| | | WORK ZONE IMPACT ATTENUATOR (UNIDIRECTIONAL) | LEO HOURS | | WORK ZONE EDGE LINE (YELLOW), CLASS III, 740.06, TYPE I | WORK ZONE EDGE LINE (WHITE), CLASS III, 740.06, TYPE I | WORK ZONE CHANNELIZING LINE, CLASS III, 740.06, TYPE I | | RUMBLE STRIPS (ASPHALT CONCRETE), AS PER PLAN | RUMBLE STRIPS, (ASPHALT CONCRETE) | | PORTABLE BARRIER, 32" | | EDGE LINE, 6" | LANE LINE, 6" | |
| | | EACH | | | MILE | MILE | FT | | FT | FT | | FT | | MILE | MILE | |
| MOT-4-18.42 L | STAGE 1 | | 16 | | | 0.17 | | | | | | | | | 0.04 | |
| | STAGE 2 | | 16 | | | 0.21 | | | | | | | | | 0.03 | |
| | STAGE 3 | | | | | | | | | | | | | | | |
| MOT-75-02.59 | STAGE 1 | 1 | 88 | | 0.44 | 0.44 | 4596 | | 2298 | 2298 | | 450 | | | | |
| MOT-675-00.55L | STAGE 2 | 1 | 88 | | 0.57 | 0.57 | 6060 | | 2890 | 2890 | | 520 | | 1.15 | 1.15 | |
| | STAGE 3 | 1 | 224 | | 0.12 | 0.12 | | | | | | 600 | | 0.23 | | |
| | STAGE 4 | 1 | 88 | | 0.57 | 0.57 | 6000 | | 3000 | 3000 | | 470 | | | | |
| | STAGE 5 | 1 | 88 | | 0.41 | 0.41 | 4360 | | 2180 | 2180 | | 400 | | 1.14 | 1.14 | |
| MOT-75-02.61 | STAGE 1 | 2 | 64 | | 0.77 | 0.83 | 4751 | | | | | 2050 | | | | |
| MOT-675-00.63Y | STAGE 2 | 2 | 32 | | | 0.61 | 3040 | | | | | 1380 | | 0.88 | 0.88 | |
| MOT-675-00.63L | | | | | | | | | | | | | | | | |
| MOT-675-00.63R | | | | | | | | | | | | | | | | |
| MOT-675-02.33 | STAGE 1 | 2 | 32 | | 1.09 | 0.97 | 5140 | | 5340 | 5340 | | 960 | | | | |
| | STAGE 2 | 2 | 144 | | | | | | | | | 440 | | 1.02 | 0.98 | |
| | | | | | | | | | | | | | | | | |
| | TOTAL | 13 | 880 | | 3.97 | 4.9 | 33947 | | 15708 | 15708 | | 7270 | | 4.42 | 4.22 | |

MAINTENANCE OF TRAFFIC SUB-SUMMARY

CALCULATED
MEF
CHECKED

GENERAL NOTES

MOT-BP-FY18

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ITEM 614, MAINTAINING TRAFFIC

PROPOSED WORK SHALL BE PERFORMED ON STRUCTURES:

MOT-4-18.42L (OVER STANLEY AVE.) - SFN 5700787
MOT-4-18.42R (OVER STANLEY AVE.) - SFN 5700817
MOT-75-02.59 (IR-675 RAMP "U" OVER IR-75) - SFN 5706432
MOT-75-02.61 (IR-675 RAMP "U" OVER SR-741) - SFN 5710782
MOT-675-00.55L (IR-675 RAMP "S" OVER IR-75) - SFN 5710707
MOT-675-00.63Y (IR-675 RAMP "Y" OVER SR-741) - SFN 5710731
MOT-675-00.63L (IR-675 OVER SR-741) - SFN 5710766
MOT-675-00.63R (IR-675 RAMP "V" OVER SR-741) - SFN 5710804
MOT-675-02.33 (YANKEE STREET OVER IR-675) - SFN 5711126

VEHICLES AND OTHER EQUIPMENT SHALL NOT BE PERMITTED TO STOP OR TO BE PARKED ALONG THE ROADWAY EXCEPT WITHIN DESIGNATED WORK AREAS AND SHALL NOT ENTER OR LEAVE WORK AREAS IN A MANNER WHICH WILL BE HAZARDOUS TO, OR INTERFERE WITH THE NORMAL FLOW OF TRAFFIC. PERSONAL VEHICLES WILL NOT BE PERMITTED TO PARK WITHIN THE RIGHT-OF-WAY EXCEPT WITHIN SPECIFIC AREAS DESIGNATED BY THE ENGINEER. NO EQUIPMENT OR MATERIALS SHALL BE STORED WITHIN THE RIGHT-OF-WAY OF ANY INTERSECTING STREET WITHOUT PRIOR WRITTEN PERMISSION.

BEFORE WORK BEGINS, THE CONTRACTOR SHALL SUBMIT TO THE ENGINEER THE NAMES AND TELEPHONE NUMBERS OF PERSONS WHO CAN BE CONTACTED 24 HOURS A DAY BY THE OHIO DEPARTMENT OF TRANSPORTATION, THE HIGHWAY PATROL, AND ALL OTHER INTERESTED POLICE AGENCIES.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIRING AND/OR REPLACING ALL TRAFFIC CONTROL DEVICES NEEDED TO MAINTAIN SAFETY FOR THE DURATION OF THIS PROJECT. THE CONTRACTOR SHALL HAVE AVAILABLE ALL MATERIALS, EQUIPMENT, AND INCIDENTALS NECESSARY TO PERFORM THE REQUIRED REPAIRS WITHIN A REASONABLE PERIOD OF TIME.

LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

THE CONTRACTOR SHALL PROVIDE, ERECT AND MAINTAIN SIGNS AND SIGN SUPPORTS, AS DETAILED IN THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES, AND TYPE III BARRICADES OF THE TYPE AND LOCATION AS SHOWN IN THE PLANS.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH CMS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

NO WORK SHALL BE PERFORMED AND ALL EXISTING LANES SHALL BE BE OPEN TO TRAFFIC DURING THE FOLLOWING DESIGNATED HOLIDAYS OR EVENTS:

| | |
|--------------|----------------|
| CHRISTMAS | FOURTH OF JULY |
| NEW YEARS | LABOR DAY |
| MEMORIAL DAY | THANKSGIVING |

THE PERIOD OF TIME THAT THE LANES ARE TO BE OPEN DEPENDS ON THE DAY OF THE WEEK ON WHICH THE HOLIDAY OR EVENT FALLS. THE FOLLOWING SCHEDULE SHALL BE USED TO DETERMINE THIS PERIOD:

| DAY OF HOLIDAY OR EVENT | TIME ALL LANES MUST BE OPEN TO TRAFFIC |
|------------------------------|---|
| SUNDAY | 12:00N FRIDAY THROUGH 6:00 AM MONDAY |
| MONDAY | 12:00N FRIDAY THROUGH 6:00 AM TUESDAY |
| TUESDAY | 12:00N MONDAY THROUGH 6:00 AM WEDNESDAY |
| WEDNESDAY | 12:00N TUESDAY THROUGH 6:00 AM THURSDAY |
| THURSDAY | 12:00N WEDNESDAY THROUGH 6:00 AM FRIDAY |
| THURSDAY (THANKSGIVING ONLY) | 12:00N WEDNESDAY THROUGH 6:00 AM MONDAY |
| FRIDAY | 12:00N THURSDAY THROUGH 6:00 AM MONDAY |
| SATURDAY | 12:00N FRIDAY THROUGH 6:00 AM MONDAY |

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE AS SHOWN BELOW FOR EACH MINUTE THE ABOVE DESCRIBED LANE CLOSURE RESTRICTIONS ARE VIOLATED.

| DESCRIPTION OF CRITICAL LANE/RAMP TO BE MAINTAINED | RESTRICTED TIME PERIOD | TIME UNIT | DISINCENTIVE \$ PER TIME UNIT |
|---|---------------------------|-----------|----------------------------------|
| STANLEY AVE | SEE ABOVE | MINUTE | \$50 |
| SR - 741 | SEE ABOVE | MINUTE | \$50 |
| IR - 75 | SEE ABOVE | MINUTE | \$100 |
| IR - 675 | SEE ABOVE | MINUTE | \$100 |

PERMITTED LANE CLOSURE

LANES CLOSURES SHALL ONLY BE IMPLEMENTED AT THE TIMES LISTED ON THE OHIO DEPARTMENT OF TRANSPORTATION'S PERMITTED LANE CLOSURE WEB SITE WHICH IS LOCATED AT:

<http://plcm.dot.state.oh.us>

THE PERMITTED CLOSURE TIMES LISTED ON THE WEBSITE, 14 CALENDAR DAYS PRIOR TO THE BID LETTING DATE, SHALL BE IN EFFECT FOR THIS PROJECT UNLESS TIMES ARE DETAILED IN THE PLANS.

NO WORK WITHIN ACTIVE TRAVEL LANES OR WHICH WILL SLOW TRAFFIC IS PERMITTED AT ANY OTHER TIMES.

THE LANE CLOSURE TIMES ON IR 75 AND IR 675 WILL BE AS FOLLOWS: ONE LANE MAY BE CLOSED FROM 7:00 PM TO 6:00 AM, SEVEN DAYS PER WEEK. TWO LANE CLOSURES WILL BE AS SPECIFIED IN ODOT'S PERMITTED LANE CLOSURE SCHEDULE LOCATED AT : <http://plcm.dot.state.oh.us>

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE AS SHOWN BELOW FOR EACH MINUTE THE ABOVE DESCRIBED LANE CLOSURE RESTRICTIONS ARE VIOLATED.

| DESCRIPTION OF CRITICAL LANE/RAMP TO BE MAINTAINED | RESTRICTED TIME PERIOD | TIME UNIT | DISINCENTIVE \$ PER TIME UNIT |
|---|---------------------------|-----------|----------------------------------|
| STANLEY AVE | SEE ABOVE | MINUTE | \$50 |
| SR - 741 | SEE ABOVE | MINUTE | \$50 |
| IR - 75 | SEE ABOVE | MINUTE | \$100 |
| IR - 675 | SEE ABOVE | MINUTE | \$100 |

NOTIFICATION OF TRAFFIC RESTRICTIONS

THROUGHOUT THE DURATION OF THE PROJECT, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER IN WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITTEN NOTIFICATION IS SUBMITTED IN A TIMELY MANNER TO ALLOW THE PROJECT ENGINEER TO MEET THE REQUIRED TIME FRAMES SET FORTH IN THE TABLE BELOW TO INFORM SPECIAL HAULING PERMITS SECTION (HAULING.PERMITS@DOT.OHIO.GOV) AND THE DISTRICT PUBLIC INFORMATION OFFICE (PIO). THIS NOTIFICATION SHALL BE RECEIVED BY THE PROJECT ENGINEER PRIOR TO THE PHYSICAL SETUP OF ANY APPLICABLE SIGNS OR MESSAGE BOARDS.

INFORMATION SHOULD INCLUDE, BUT IS NOT LIMITED TO, ALL CONSTRUCTION ACTIVITIES THAT IMPACT OR INTERFERE WITH TRAFFIC AND SHALL LIST THE SPECIFIC LOCATION, TYPE OF WORK, ROAD STATUS, DATE AND TIME OF RESTRICTION, DURATION OF RESTRICTION, NUMBER OF LANES MAINTAINED, NUMBER OF LANES CLOSED, MINIMUM WIDTH OF DRIVABLE PAVEMENT, DETOUR ROUTES, IF APPLICABLE, AND ANY OTHER INFORMATION REQUESTED BY THE PROJECT ENGINEER.

EXISTING VERTICAL CLEARANCE OVER LIVE TRAFFIC SHALL BE MAINTAINED AT ALL TIMES.

| NOTIFICATION TIME TABLE | | |
|--|---------------------------|--|
| ITEM | DURATION OF CLOSURE | NOTICE DUE TO D7 PERMITS & PIO |
| RAMP & ROAD CLOSURES | >= 2 WKS | 21 CALENDAR DAYS PRIOR TO CLOSURE |
| | > 12 HRS & < 2 WKS | 14 CALENDAR DAYS PRIOR TO CLOSURE |
| | < 12 HRS | 4 BUSINESS DAYS PRIOR TO CLOSURE |
| LANE CLOSURES AND RESTRICTIONS | >= 2 WKS | 14 CALENDAR DAYS PRIOR TO CLOSURE |
| | < 2 WKS | 5 BUSINESS DAYS PRIOR TO CLOSURE |
| START OF CONSTRUCTION & TRAFFIC PATTERN CHANGES | N/A | 14 CALENDAR DAYS PRIOR TO IMPLEMENTATION |

ANY UNFORESEEN CONDITIONS NOT SPECIFIED IN THE PLANS REQUIRING TRAFFIC RESTRICTIONS SHALL ALSO BE REPORTED TO THE PROJECT ENGINEER USING THE NOTIFICATION TIME TABLE.

ITEM 614, MAINTAINING TRAFFIC (NOTICE OF CLOSURE SIGN)

NOTICE OF CLOSURE SIGNS (W20-H13), SHALL BE ERECTED BY THE CONTRACTOR PRIOR TO THE SCHEDULED ROAD OR RAMP CLOSURE IN ACCORDANCE WITH THE NOTICE OF CLOSURE TIME TABLE BELOW. <<AT THE APPROVAL OF THE ENGINEER, PORTABLE CHANGABLE MESSAGE SIGNS MAY BE USED IN LIEU OF THE STANDARD FLATSHEET SIGN FOR CLOSURE DURATIONS OF LESS THAN 1 WEEK.>>

THE SIGNS SHALL BE ERECTED ON THE RIGHT-HAND SIDE OF THE ROAD/RAMP FACING TRAFFIC. THEY SHALL BE PLACED SO AS NOT TO INTERFERE WITH THE VISIBILITY OF ANY OTHER TRAFFIC CONTROL SIGNS. ON ROADWAYS, THEY SHOULD BE ERECTED AT OR NEAR THE POINT OF CLOSURE. THE SIGNS MAY BE ERECTED ANYWHERE ON RAMPS AS LONG AS THEY ARE VISIBLE TO THE MOTORISTS USING THE RAMP. ON ENTRANCE RAMPS, THE SIGN SHALL BE ERECTED WELL IN ADVANCE OF THE MERGE AREA TO AVOID DISTRACTING MOTORISTS.

| NOTICE OF CLOSURE SIGN TIME TABLE | | |
|-----------------------------------|------------------------|--------------------------------------|
| ITEM | DURATION OF CLOSURE | SIGN DISPLAYED TO PUBLIC |
| RAMP & ROAD CLOSURES | >= 2 WKS | 14 CALENDAR DAYS PRIOR TO CLOSURE |
| | > 12 HRS & < 2 WKS | 7 CALENDAR DAYS PRIOR TO CLOSURE |
| | < 12 HRS | 2 BUSINESS DAYS PRIOR TO CLOSURE |

THE SIGN SHALL DISPLAY THE DATE OF THE CLOSURE IN MM-DD FORMAT AND THE NUMBER OF DAYS OF THE CLOSURE. THE LAST LINE OF THE W20-H13 SIGN LISTS A PHONE NUMBER WHICH A MOTORIST MAY CALL FOR ADDITIONAL INFORMATION. THIS NUMBER SHALL BE 888-200-9919.

ITEM 614, MAINTAINING TRAFFIC (ROAD CLOSED SIGN)

THE CONTRACTOR SHALL PROVIDE, ERECT AND MAINTAIN STANDARD 48 X 30 INCH ROAD CLOSED SIGNS, SIGN SUPPORTS, BARRICADES AND LIGHTS, AS DETAILED IN SCD MT-101.60 AT THE FOLLOWING LOCATIONS DURING PERIODS IN WHICH THE AFFECTED ROADS ARE CLOSED TO TRAFFIC.

STANLEY AVENUE AT THE SR 4 INTERCHANGE

ITEM 614, MAINTAINING TRAFFIC (SIGNS AND BARRICADES)

THE CONTRACTOR SHALL PROVIDE, ERECT AND MAINTAIN SIGNS AND SIGN SUPPORTS, AS DETAILED IN THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES, AND TYPE III BARRICADES OF THE TYPE AND LOCATION AS FOLLOWS:

STANLEY AVENUE AT THE SR 4 INTERCHANGE

SPEED MEASUREMENT MARKINGS

THE CONTRACTOR SHALL REMOVE EIGHT AIR SPEED MARKINGS ON IR-75 AT IR-675 AS SHOWN ON SHEET -- DURING THE MAINTENANCE OF TRAFFIC. THE CONTRACTOR SHALL RESTORE THE AIR SPEED MARKINGS AFTER CONSTRUCTION IS COMPLETED. THE AIR SPEED MARKINGS SHALL BE WHITE, 24 INCHES IN THE DIRECTION OF TRAVEL AND 4 FEET IN LENGTH. THE AIR SPEED ZONE MARKINGS SHALL BE PLACED ENTIRELY ON THE SHOULDER. THE CONTRACTOR SHALL RESTORE THE MARKINGS SO THAT THEY ARE PLACED AT 0.25 MILE INTERVALS BEGINNING WITH THE EXISTING AIR SPEED ZONE MARKINGS SOUTH OF THE PROJECT.

IT IS THE CONTRACTOR'S RESPONSIBILITY TO HAVE THE MARKINGS LAID OUT BY A REGISTERED SURVEYOR. A RECORD IS TO BE KEPT. ONE ORIGINAL SIGNED AND SEALED DOCUMENT IS TO BE SENT TO THE DISTRICT TRAFFIC ENGINEER AND ONE COPY IS TO BE SENT TO THE DISTRICT CONSTRUCTION ADMINISTRATOR.

PAYMENT SHALL INCLUDE THE REMOVAL, SURVEY WORK AND REINSTALLATION OF THE AIR SPEED MARKINGS. THE EIGHT AIR SPEED MARKINGS DISTURBED AND REPLACED, WILL BE MEASURED AS 1 EACH AIR SPEED MARKING.

ITEM 614 - PORTABLE BARRIER, 32"

PORTABLE BARRIER AS DETAILED IN THE PLANS SHALL NOT BE NON-PERFORMED IN THE FIELD.

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SEQUENCE OF CONSTRUCTION

MOT-4-18.42L (OVER STANLEY AVE.) - SFN 5700787
MOT-4-18.42R (OVER STANLEY AVE.) - SFN 5700817

STAGE 1: REDUCE IN WIDTH AND SHIFT TRAFFIC TO THE LEFT LANE. CLOSE THE RIGHT LANE OF BOTH NORTHBOUND AND SOUTHBOUND STANLEY AVE. WITH DRUMS FOR A PERIOD NOT TO EXCEED 14 CONSECUTIVE CALENDAR DAYS FOR EITHER DIRECTION. PEDESTRIANS SHALL BE DETOURED PER STANDARD DRAWING MT-110.10.

STAGE 2: REDUCE IN WIDTH AND SHIFT TRAFFIC TO THE RIGHT LANE. CLOSE THE LEFT LANE OF BOTH NORTHBOUND AND SOUTHBOUND STANLEY AVE. WITH DRUMS FOR A PERIOD NOT EXCEED 21 CONSECUTIVE CALENDAR DAYS.

STAGE 3: THE STRIP AREA NOT COVERED IN PHASE 1 OR 2 SHALL BE PAINTED UNDER CLOSURE. DETOURS SHALL BE PROVIDED AS DETAILED IN THE PLANS. STANLEY AVE MAY ONLY BE CLOSED FROM FRIDAY AT 8:00 PM TO MONDAY AT 6:00 AM. STANLEY AVE MAY ONLY BE CLOSED FOR TWO WEEKENDS PER TRAVELED DIRECTION.

MOT-75-02.59 (IR-675 RAMP "U" OVER IR-75) - SFN 5706432
MOT-675-00.55L (IR-675 RAMP "S" OVER IR-75) - SFN 5710707

STAGE 1: ALL THREE LANES OF IR-75 SOUTHBOUND SHALL BE REDUCED IN WIDTH AND SHIFTED TO THE INSIDE AS DETAILED IN THE PLANS. THE CONTRACTOR SHALL PAINT WITHIN THE CLOSURE AND SHOULDERS ON BOTH STRUCTURES FOR A PERIOD NOT TO EXCEED 28 CONSECUTIVE CALENDAR DAYS. THE OUTSIDE SHOULDER AND LANE SHALL BE CLOSED USING PORTABLE BARRIER FOR PROTECTION OF THE EQUIPMENT AND WORKERS FOR THE DURATION OF STAGE 1.

UNDER THE STAGE 1 CONFIGURATION, THE CONTRACTOR SHALL CLOSE THE ADJACENT LANE PER STANDARD DRAWING MT-95.30. THE SHADOW VEHICLE SHALL BE EQUIPPED WITH A TRUCK-MOUNTED ATTENUATOR. ALL LANE CLOSURES SHALL BE DONE USING PERMITTED LANE CLOSURES.

STAGE 2: ALL THREE LANES OF IR-75 SOUTHBOUND SHALL BE REDUCED IN WIDTH AND SHIFTED TO THE OUTSIDE AS DETAILED IN THE PLANS. THE CONTRACTOR SHALL PAINT WITHIN THE CLOSURE AND SHOULDERS ON BOTH STRUCTURES FOR A PERIOD NOT TO EXCEED 28 CONSECUTIVE CALENDAR DAYS. THE INSIDE SHOULDER AND LANE SHALL BE CLOSED USING PORTABLE BARRIER FOR PROTECTION OF THE EQUIPMENT AND WORKERS FOR THE DURATION OF STAGE 2.

UNDER THE STAGE 2 CONFIGURATION, THE CONTRACTOR SHALL CLOSE THE ADJACENT LANE PER STANDARD DRAWING MT-95.30. THE SHADOW VEHICLE SHALL BE EQUIPPED WITH A TRUCK-MOUNTED ATTENUATOR. ALL LANE CLOSURES SHALL BE DONE USING PERMITTED LANE CLOSURES.

STAGE 3: ALL LANES OF IR-75 SHALL BE MAINTAINED IN THEIR ORIGINAL LOCATION. THE CONTRACTOR MAY PAINT BOTH STRUCTURES, INCLUDING THE STEEL PIER CAP FROM WITHIN THE MEDIAN. THE CONTRACTOR MAY CLOSE A LANE ON THE MOT-675-0055L STRUCTURE PER STANDARD DRAWING MT-95.30. THE SHADOW VEHICLE SHALL BE EQUIPPED WITH A TRUCK-MOUNTED ATTENUATOR. ALL WORK IN THE MEDIAN SHALL TAKE PLACE BEHIND THE EXISTING GUARDRAIL FOR PROTECTION OF THE EQUIPMENT AND WORKERS. STAGE 3 SHALL NOT EXCEED 35 CONSECUTIVE CALENDAR DAYS.

STAGE 4: ALL THREE LANES OF IR-75 NORTHBOUND SHALL BE REDUCED IN WIDTH AND SHIFTED TO THE OUTSIDE AS DETAILED IN THE PLANS. THE CONTRACTOR SHALL PAINT WITHIN THE CLOSURE AND SHOULDERS ON BOTH STRUCTURES FOR A PERIOD NOT TO EXCEED 28 CONSECUTIVE CALENDAR DAYS. THE INSIDE SHOULDER AND LANE SHALL BE CLOSED USING PORTABLE BARRIER FOR PROTECTION OF THE EQUIPMENT AND WORKERS FOR THE DURATION OF STAGE 4.

UNDER THE STAGE 4 CONFIGURATION, THE CONTRACTOR SHALL CLOSE THE ADJACENT LANE PER STANDARD DRAWING MT-95.30. THE SHADOW VEHICLE SHALL BE EQUIPPED WITH A TRUCK-MOUNTED ATTENUATOR. ALL LANE CLOSURES SHALL BE DONE USING PERMITTED LANE CLOSURES.

STAGE 5: ALL THREE LANES OF IR-75 NORTHBOUND SHALL BE REDUCED IN WIDTH AND SHIFTED TO THE INSIDE AS DETAILED IN THE PLANS. THE CONTRACTOR SHALL PAINT WITHIN THE CLOSURE AND SHOULDERS ON BOTH STRUCTURES FOR A PERIOD NOT TO EXCEED 28 CONSECUTIVE CALENDAR DAYS. THE OUTSIDE SHOULDER AND LANE SHALL BE CLOSED USING PORTABLE BARRIER FOR PROTECTION OF THE EQUIPMENT AND WORKERS FOR THE DURATION OF STAGE 5.

UNDER THE STAGE 5 CONFIGURATION, THE CONTRACTOR SHALL CLOSE THE ADJACENT LANE PER STANDARD DRAWING MT-95.30. THE SHADOW VEHICLE SHALL BE EQUIPPED WITH A TRUCK-MOUNTED ATTENUATOR. ALL LANE CLOSURES SHALL BE DONE USING PERMITTED LANE CLOSURES.

◊◊ - OVERHEADED ACCESS TO THE MOT-75-0259 STRUCTURE IS POSSIBLE BY SHIFTING THE RAMP LANE PER MT-102.10. THE UNDERSIDE OF THE BRIDGE SHALL BE ACCESSED ENTIRELY FROM THE INSIDE SHOULDER CLOSURE. THE IMPACT ATTENUATOR SHALL NOT BE MOUNTED TO THE BRIDGE DECK. SEE SHEET 21/71 FOR TRANSVERSE SECTION DETAIL.

MOT-675-00.63Y (IR-675 RAMP "Y" OVER SR-741) - SFN 5710731
MOT-675-00.63L (IR-675 OVER SR-741) - SFN 5710766
MOT-75-02.61 (IR-675 RAMP "U" OVER SR-741) - SFN 5710782
MOT-675-00.63R (IR-675 RAMP "V" OVER SR-741) - SFN 5710804

STAGE 1: BOTH LANES OF NORTHBOUND AND SOUTHBOUND SR 741 SHALL BE REDUCED IN WIDTH AND SHIFTED TO THE OUTSIDE AS DETAILED IN THE PLANS. ALL WORK SHALL TAKE PLACE BEHIND PORTABLE BARRIER FOR PROTECTION OF THE EQUIPMENT AND WORKERS FOR THE DURATION OF STAGE 1. THE CONTRACTOR SHALL PAINT WITHIN THE CLOSURE FOR A PERIOD NOT TO EXCEED 28 CONSECUTIVE CALENDAR DAYS.

UNDER THE STAGE 1 CONFIGURATION, THE CONTRACTOR SHALL CLOSE THE ADJACENT LANE PER STANDARD DRAWING MT-95.30. THE SHADOW VEHICLE SHALL BE EQUIPPED WITH A TRUCK-MOUNTED ATTENUATOR. THE ADJACENT LANE MAY ONLY BE CLOSED AT NIGHT FROM 8:00 PM TO 6:00 AM.

STAGE 2: BOTH LANES OF NORTHBOUND AND SOUTHBOUND SR 741 SHALL BE REDUCED IN WIDTH AND SHIFTED TO THE INSIDE AS DETAILED IN THE PLANS. ALL WORK SHALL TAKE PLACE BEHIND PORTABLE BARRIER FOR PROTECTION OF THE EQUIPMENT AND WORKERS FOR THE DURATION OF STAGE 2. THE CONTRACTOR SHALL PAINT WITHIN THE CLOSURE FOR A PERIOD NOT TO EXCEED 28 CONSECUTIVE CALENDAR DAYS.

MOT-675-02.33 (YANKEE STREET OVER IR-675) - SFN 5711126

STAGE 1: TWO LANES OF BOTH NORTHBOUND AND SOUTHBOUND IR-675 SHALL BE REDUCED IN WIDTH AND SHIFTED TO THE OUTSIDE AS DETAILED IN THE PLANS. THE CONTRACTOR SHALL PAINT WITHIN THE CLOSURE AND THE MEDIAN FOR A PERIOD NOT TO EXCEED 28 CONSECUTIVE CALENDAR DAYS. THE INSIDE SHOULDER SHALL BE CLOSED USING PORTABLE BARRIER FOR PROTECTION OF THE EQUIPMENT AND WORKERS FOR THE DURATION OF STAGE 1. ALL LANE CLOSURES SHALL BE DONE USING PERMITTED LANE CLOSURES.

DURING THE STAGE 1 CONFIGURATION, THE CONTRACTOR SHALL CLOSE THE IR 675 NORTHBOUND ON RAMP FROM EASTBOUND SR 725 AND DETOUR THEM AS SHOWN IN THE PLANS. THE PROJECT ENGINEER SHALL NOTIFY THE DISTRICT 7 TRAFFIC ENGINEER 7 DAYS PRIOR TO THE RAMP BEING CLOSED AND WHEN THE RAMP IS OPEN.

STAGE 2: THE OUTSIDE SHOULDER SHALL BE CLOSED PER STANDARD DRAWING MT-95.45. EQUIPMENT SHALL BE PLACED BEHIND THE PORTABLE CONCRETE BARRIER FOR PROTECTION. THE CONTRACTOR SHALL PAINT WITHIN THE CLOSURE FOR A PERIOD NOT TO EXCEED 28 CONSECUTIVE CALENDAR DAYS.

UNDER THE STAGE 2 CONFIGURATION, THE CONTRACTOR SHALL CLOSE THE ADJACENT LANE PER STANDARD DRAWING MT-95.30. THE SHADOW VEHICLE SHALL BE EQUIPPED WITH A TRUCK-MOUNTED ATTENUATOR. ALL LANE CLOSURES SHALL BE DONE USING PERMITTED LANE CLOSURES.

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FLOODLIGHTING

FLOODLIGHTING OF THE WORK SITE FOR OPERATIONS CONDUCTED DURING NIGHTTIME PERIODS SHALL BE ACCOMPLISHED SO THAT THE LIGHTS DO NOT CAUSE GLARE TO THE DRIVERS ON THE ROADWAY. TO ENSURE THE ADEQUACY OF THE FLOODLIGHT PLACEMENT, THE CONTRACTOR AND THE ENGINEER SHALL DRIVE THROUGH THE WORK SITE EACH NIGHT WHEN THE LIGHTING IS IN PLACE AND OPERATIVE PRIOR TO COMMENCING ANY WORK. IF GLARE IS DETECTED, THE LIGHT PLACEMENT AND SHIELDING SHALL BE ADJUSTED TO THE SATISFACTION OF THE ENGINEER BEFORE WORK PROCEEDS.

PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC.

ITEM 614, REPLACEMENT SIGN

FLATSHEET SIGNS FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH THE REQUIREMENTS OF THE PLANS, SPECIFICATIONS AND PROPOSAL WHICH BECOME DAMAGED BY TRAFFIC FOR REASONS BEYOND THE CONTROL OF THE CONTRACTOR SHALL BE REPLACED IN KIND WHEN ORDERED BY THE ENGINEER. REPLACEMENT SIGNS SHALL BE NEW. OTHER MATERIALS MAY BE IN USED, BUT GOOD, CONDITION SUBJECT TO APPROVAL BY THE ENGINEER.

PAYMENT FOR THE NEW SIGNS SHALL BE MADE AT THE CONTRACT PRICE PER EACH FOR ITEM 614, REPLACEMENT SIGN, AND SHALL INCLUDE THE COST OF REMOVING AND DISPOSING OF DAMAGED SIGNS, HARDWARE AND SUPPORTS, AND PROVIDING THE NECESSARY REPLACEMENT HARDWARE, SUPPORTS, ETC.

AN ESTIMATED QUANTITY OF 4 EACH HAS BEEN PROVIDED IN THE GENERAL SUMMARY.

ITEM 614, REPLACEMENT DRUM

DRUMS FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH THE REQUIREMENTS OF THE PLANS, SPECIFICATIONS AND PROPOSAL WHICH BECOME DAMAGED BY TRAFFIC FOR REASONS BEYOND THE CONTROL OF THE CONTRACTOR SHALL BE REPLACED IN KIND WHEN ORDERED BY THE ENGINEER. REPLACEMENT DRUMS SHALL BE NEW.

PAYMENT FOR THE NEW DRUMS SHALL BE MADE AT THE CONTRACT PRICE PER EACH FOR ITEM 614, REPLACEMENT DRUM, AND SHALL INCLUDE THE COST OF REMOVING AND DISPOSING OF THE DAMAGED DRUM, AND PROVIDING AND MAINTAINING THE REPLACEMENT DRUM IN ACCORDANCE WITH THE CONTRACT REQUIREMENTS FOR THE ORIGINAL DRUM.

AN ESTIMATED QUANTITY OF 10 EACH HAS BEEN PROVIDED IN THE GENERAL SUMMARY.

ITEM 618 - RUMBLE STRIPS (ASPHALT CONCRETE), AS PER PLAN

THIS ITEM IS FOR REMOVAL OF EXISTING RUMBLE STRIPS FOR MAINTENANCE OF TRAFFIC PURPOSES ONLY. THE CONTRACTOR SHALL MILL 2 INCHES DEEP BY 2 FEET WIDE OF THE EXISTING ASPHALT SHOULDER IN ORDER TO REMOVE THE EXISTING RUMBLE STRIPS ALONG DETAILED ROUTES IN THE AREA WHERE TRAFFIC IS SHIFTED. THE CONTRACTOR SHALL THEN COAT ALL MILLED SURFACES, HORIZONTAL AND VERTICAL, WITH APPROVED AC LIQUID. NEXT THE CONTRACTOR SHALL PLACE 2 INCHES OF ITEM 448 ASPHALT CONCRETE SURFACE COURSE, TYPE 1, PG 64-28. THE COST OF THE REMOVAL OF THE EXISTING PAVEMENT, AND PLACEMENT OF THE SURFACE COURSE SHALL BE INCLUDED IN UNIT PRICE BID PER FOOT OF ITEM 618 - RUMBLE STRIPS (ASPHALT CONCRETE), AS PER PLAN.

AN ESTIMATED QUANTITY OF 15708 FEET HAS BEEN CARRIED TO THE MAINTENANCE OF TRAFFIC SUBSUMMARY.

ITEM 614, WORK ZONE IMPACT ATTENUATOR FOR 24" WIDE HAZARDS UNIDIRECTIONAL

THIS ITEM SHALL CONSIST OF FURNISHING AND INSTALLING A NON-GATING IMPACT ATTENUATOR. FURNISH AN IMPACT ATTENUATOR FROM THE OFFICE OF ROADWAY ENGINEERING APPROVED LIST FOR WORK ZONE IMPACT ATTENUATORS. THE APPROVED LIST IS AVAILABLE AT THE "ROADWAY STANDARDS: PROPRIETARY ROADSIDE SAFETY DEVICES" WEB PAGE ON THE OFFICE OF ROADWAY ENGINEERING WEBSITE.

INSTALLATION SHALL BE AT THE LOCATIONS SPECIFIED IN THE PLANS IN ACCORDANCE WITH THE MANUFACTURER'S SPECIFICATIONS.

THE CONTRACTOR SHALL REPAIR OR REPLACE A DAMAGED UNIT WITHIN 24 HOURS OF A DAMAGING IMPACT.

WHEN GATING IMPACT ATTENUATORS ARE DESIRED, THE CONTRACTOR SHALL SUBMIT DOCUMENTATION TO THE ENGINEER FOR ACCEPTANCE.

IMPACT ATTENUATORS SHALL NOT BE MOUNTED TO THE BRIDGE DECK.

THE COST FOR THE ADDITIONAL BARRIER REQUIRED FOR A GATING IMPACT ATTENUATOR SHALL BE INCLUDED IN THE COST OF THE GATING IMPACT ATTENUATOR.

PAYMENT FOR THE ABOVE WORK SHALL BE MADE AT THE UNIT PRICE BID AND SHALL INCLUDE ALL LABOR, TOOLS, EQUIPMENT AND MATERIALS NECESSARY TO CONSTRUCT AND MAINTAIN A COMPLETE AND FUNCTIONAL IMPACT ATTENUATOR SYSTEM, INCLUDING ALL RELATED BACKUPS, TRANSITIONS, LEVELING PADS, HARDWARE AND GRADING, NOT SEPARATELY SPECIFIED, AS REQUIRED BY THE MANUFACTURER.

ITEM 614 - LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS

USE OF LAW ENFORCEMENT OFFICERS (LEOS) BY CONTRACTORS OTHER THAN THE USES SPECIFIED BELOW WILL NOT BE PERMITTED AT PROJECT COST. LEOS SHOULD NOT BE USED WHERE THE OMUTCD INTENDS THAT FLAGGERS BE USED.

IN ADDITION TO THE REQUIREMENTS OF C&MS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHALL BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS:

DURING THE ENTIRE ADVANCE PREPARATION AND CLOSURE SEQUENCE WHERE COMPLETE BLOCKAGE OF TRAFFIC IS REQUIRED.

DURING A TRAFFIC SIGNAL INSTALLATION WHEN IMPACTING THE NORMAL FUNCTION OF THE SIGNAL OR THE FLOW OF TRAFFIC OR WHEN TRAFFIC NEEDS TO BE DIRECTED THROUGH AN ENERGIZED TRAFFIC SIGNAL CONTRARY TO THE SIGNAL DISPLAY (E.G., DIRECTING MOTORISTS THROUGH A RED LIGHT).

IN ADDITION TO THE REQUIREMENT OF C&MS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHOULD BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS AS APPROVED BY THE ENGINEER:

FOR LANE CLOSURES: DURING INITIAL SET-UP PERIODS, TEAR DOWN PERIODS, SUBSTANTIAL SHIFTS OF A CLOSURE POINT OR WHEN NEW LANE CLOSURE ARRANGEMENTS ARE INITIATED FOR LONG-TERM LANE CLOSURES/SHIFTS (FOR THE FIRST AND LAST DAY OF MAJOR CHANGES IN TRAFFIC CONTROL SETUP).

IN GENERAL, LEOS SHOULD BE POSITIONED IN ADVANCE OF AND ON THE SAME SIDE AS THE LANE RESTRICTION OR AT THE POINT OF ROAD CLOSURE, AND TO MANUALLY CONTROL TRAFFIC MOVEMENTS THROUGH SIGNALIZED INTERSECTIONS IN WORK ZONES.

LEOS SHOULD NOT FORGO THEIR TRAFFIC CONTROL RESPONSIBILITIES TO APPREHEND MOTORISTS FOR ROUTINE TRAFFIC VIOLATIONS. HOWEVER, IF A MOTORIST'S ACTIONS ARE CONSIDERED TO BE RECKLESS, THEN PURSUIT OF THE MOTORIST IS APPROPRIATE.

THE LEOS WORK AT THE DIRECTION OF THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR SECURING THE SERVICES OF THE LEOS WITH THE APPROPRIATE AGENCIES AND COMMUNICATING THE INTENTIONS OF THE PLANS WITH RESPECT TO DUTIES OF THE LEOS. THE ENGINEER SHALL HAVE FINAL CONTROL OVER THE LEOS' DUTIES AND PLACEMENT, AND WILL RESOLVE ANY ISSUES THAT MAY ARISE BETWEEN THE TWO PARTIES.

THE LEO SHALL REPORT IN TO THE CONTRACTOR PRIOR TO THE START OF THE SHIFT, IN ORDER TO RECEIVE INSTRUCTIONS REGARDING SPECIFIC WORK ASSIGNMENTS DURING HIS/HER SHIFT. THE LEO IS EXPECTED TO STAY AT THE PROJECT SITE FOR THE ENTIRE DURATION OF HIS/HER SHIFT. THE LEO SHALL REPORT TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT. ONCE THE LEO HAS COMPLETED THE DUTIES DESCRIBED ABOVE AND STILL HAS TIME REMAINING ON HIS/HER SHIFT, THE LEO MAY BE ASKED TO PATROL THROUGH THE WORK ZONE (WITH FLASHING LIGHTS OFF) OR BE PLACED AT A LOCATION TO DETER MOTORISTS FROM SPEEDING. SHOULD IT BE NECESSARY

TO LEAVE THE PROJECT SITE, THE LEO SHALL NOTIFY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE THE LEO WITH A TWO-WAY COMMUNICATION DEVICE WHICH SHALL BE RETURNED TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT.

LEOS (WITH PATROL CAR) REQUIRED BY THE TRAFFIC MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON A UNIT PRICE (HOURLY) BASIS UNDER ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE 880 HOURS

THE HOURS PAID SHALL INCLUDE ANY MINIMUM SHOW-UP TIME REQUIRED BY THE LAW ENFORCEMENT AGENCY INVOLVED.

ANY ADDITIONAL COSTS (ADMINISTRATIVE OR OTHERWISE) INCURRED BY THE CONTRACTOR TO OBTAIN THE SERVICES OF AN LEO ARE INCLUDED WITH THE BID UNIT PRICE FOR ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE.

DELINEATION OF TEMPORARY AND PERMANENT GUARDRAIL

BARRIER REFLECTORS SHALL BE INSTALLED ON ALL TEMPORARY GUARDRAIL USED FOR TRAFFIC CONTROL AND ON ALL PERMANENT GUARDRAIL LOCATED WITHIN 5 FEET OF THE EDGE OF THE ADJACENT TRAVEL LANE. BARRIER REFLECTORS SHALL CONFORM TO C&MS 626.

OBJECT MARKERS SHALL BE INSTALLED ON ALL TEMPORARY AND PERMANENT GUARDRAIL LOCATED WITHIN 5 FEET OF THE EDGE OF THE ADJACENT TRAVEL LANE. GUARDRAIL-MOUNTING OF OBJECT

MARKERS SHALL BE MADE BY INSTALLING THE OBJECT MARKERS ON THE EXTENSION BLOCKS RATHER THAN DIRECTLY ONTO THE GUARDRAIL ITSELF. OBJECT MARKERS SHALL CONFORM TO C&MS 614.03 AND THE SPACING SHALL BE APPROXIMATELY 50 FEET.

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED IN THE PLANS AND CARRIED TO THE GENERAL SUMMARY:

ITEM 614, BARRIER REFLECTOR, TYPE (2) (One-Way) 21 EACH
ITEM 614, OBJECT MARKER, ONE WAY, 21 EACH

PAYMENT SHALL BE FULL COMPENSATION FOR ALL MATERIAL, LABOR, INCIDENTALS AND EQUIPMENT NECESSARY FOR FURNISHING, INSTALLING, MAINTAINING AND REMOVING THE ABOVE ITEM(S).

DELINEATION OF PORTABLE AND PERMANENT BARRIER

BARRIER REFLECTORS AND OBJECT MARKERS SHALL BE INSTALLED ON ALL PORTABLE BARRIER (PB) USED FOR TRAFFIC CONTROL AND ON PERMANENT CONCRETE BARRIER (INCLUDING BRIDGE PARAPETS) LOCATED WITHIN 5 FEET OF THE EDGE OF THE ADJACENT TRAVEL LANE.

BARRIER REFLECTORS SHALL CONFORM TO C&MS 626, EXCEPT THAT THE SPACING SHALL BE AS PER TRAFFIC SCD MT-101.70. OBJECT MARKERS AND THEIR INSTALLATION SHALL CONFORM TO C&MS 614.03 AND SCD MT-101.70. WHEN THE PB CONTAINS GLARE SCREEN, ONE SET OF THREE VERTICAL STRIPES OF SHEETING SHALL BE CONSIDERED EQUIVALENT TO AN OBJECT MARKER, ONE-WAY.

INCREASED BARRIER DELINEATION, AS SPECIFIED HEREIN, SHALL BE INSTALLED ON ALL PB AND CONCRETE PERMANENT BARRIER LOCATED WITHIN 5 FEET OF THE EDGE OF THE TRAVELED LANE UNDER EITHER OF THE FOLLOWING CONDITIONS: ALONG TAPERS AND TRANSITION AREAS AND ALONG CURVES (OUTSIDE ONLY) WITH DEGREE OF CURVATURE GREATER THAN OR EQUAL TO 3 DEGREES.

THE INCREASED BARRIER DELINEATION SHALL CONSIST OF EITHER DELINEATION PANELS OR THE TRIPLE STACKING OF WORK ZONE BARRIER REFLECTORS.

DELINEATION PANELS SHALL CONSIST OF PANELS OF DELINEATION, APPROXIMATELY 34 INCHES LONG AND 6 INCHES WIDE AND SHALL BE "CRIMPED." PANELS SHALL BE INSTALLED AND SPACED PER TRAFFIC SCD MT-101.70.

TRIPLE-STACKED BARRIER REFLECTORS SHALL CONSIST OF ALIGNING THREE BARRIER REFLECTORS VERTICALLY, AT LOCATIONS WHERE A SINGLE BARRIER REFLECTOR WOULD BE OTHERWISE ATTACHED. THERE SHALL BE NO OPEN SPACE BETWEEN THE ADJACENT BARRIER REFLECTORS. THE TRIPLE-STACKED BARRIER REFLECTORS SHALL CONFORM TO C&MS 626, EXCEPT THAT THEY SHALL BE SPACED AND ALIGNED PER TRAFFIC SCD MT-101.70.

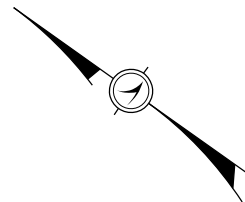
THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED IN THE PLANS AND CARRIED TO THE GENERAL SUMMARY:

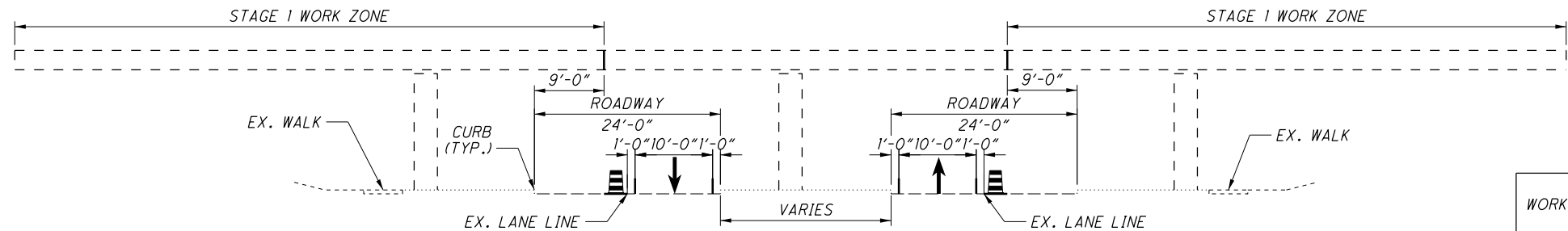
ITEM 614, BARRIER REFLECTOR, TYPE 1 (One-Way) 175 EACH

ITEM 614, OBJECT MARKER, ONE-WAY 175 EACH
ITEM 614, INCREASED BARRIER DELINEATION 2300 FEET

PAYMENT SHALL BE FULL COMPENSATION FOR ALL MATERIAL, LABOR, INCIDENTALS AND EQUIPMENT NECESSARY FOR FURNISHING, INSTALLING, MAINTAINING AND REMOVING EACH OF THE ABOVE ITEMS.

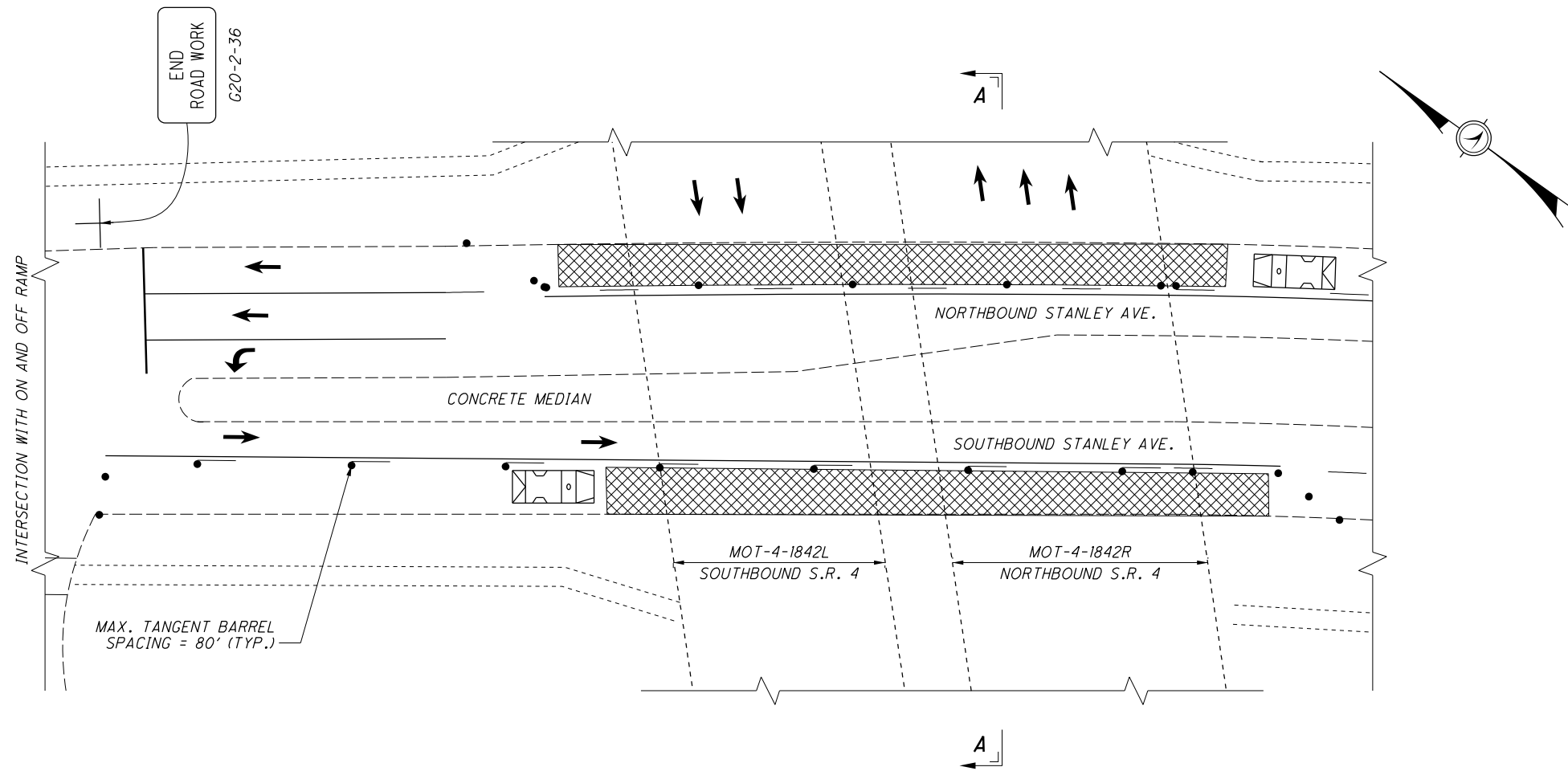
ALONG RUNS OF INCREASED BARRIER DELINEATION WHERE THIS ITEM IS PROVIDED, THE QUANTITY SHALL BE MEASURED AS THE ENTIRE LENGTH OF THE RUN OF INCREASED BARRIER DELINEATION, INCLUDING THE SPACES BETWEEN THE INDIVIDUAL DELINEATION PANELS OR STACKS OF BARRIER REFLECTORS.





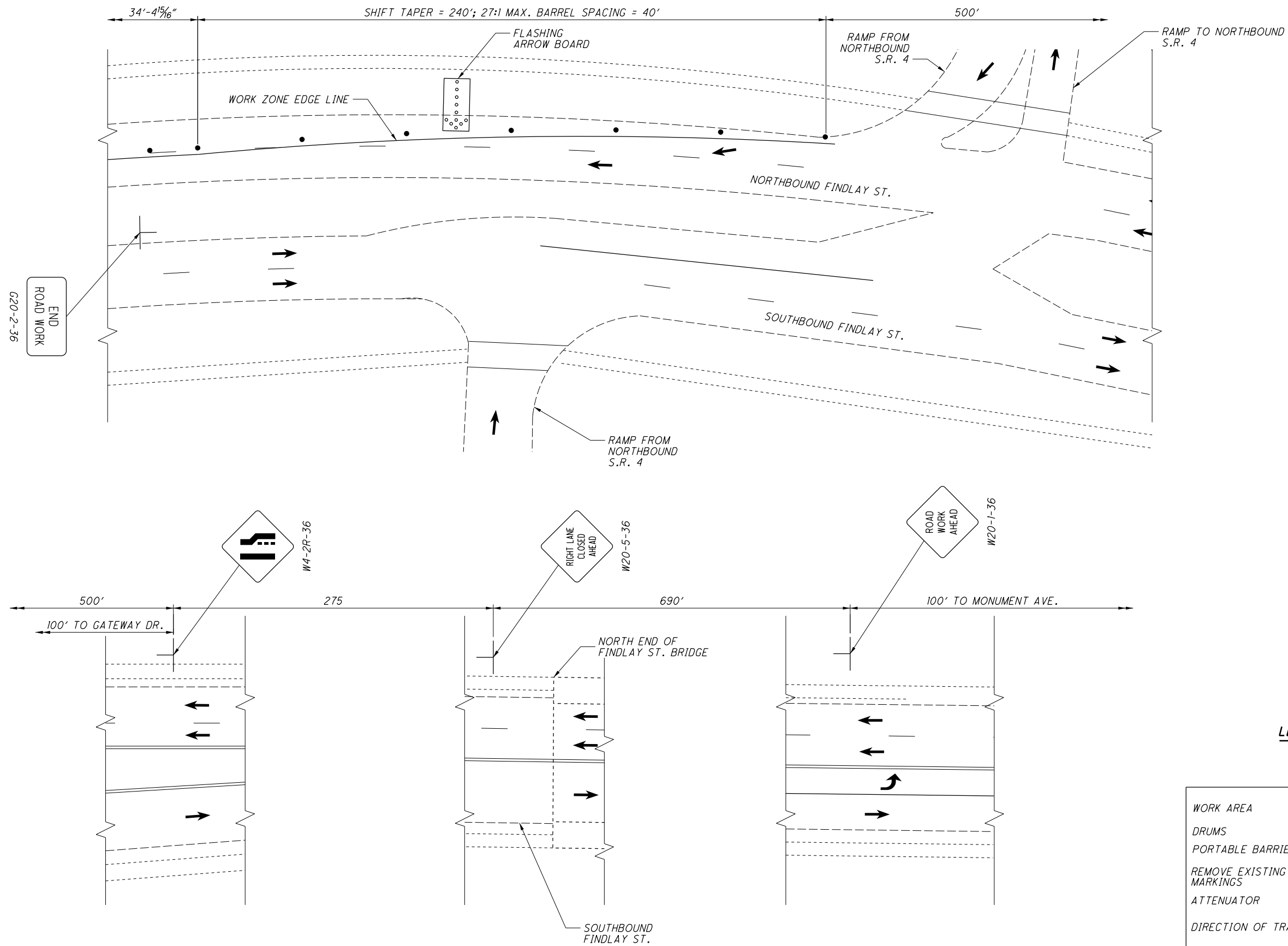
SECTION A-A (STAGE 1)

(BRIDGE ELEMENTS NOT SHOWN TO SCALE)

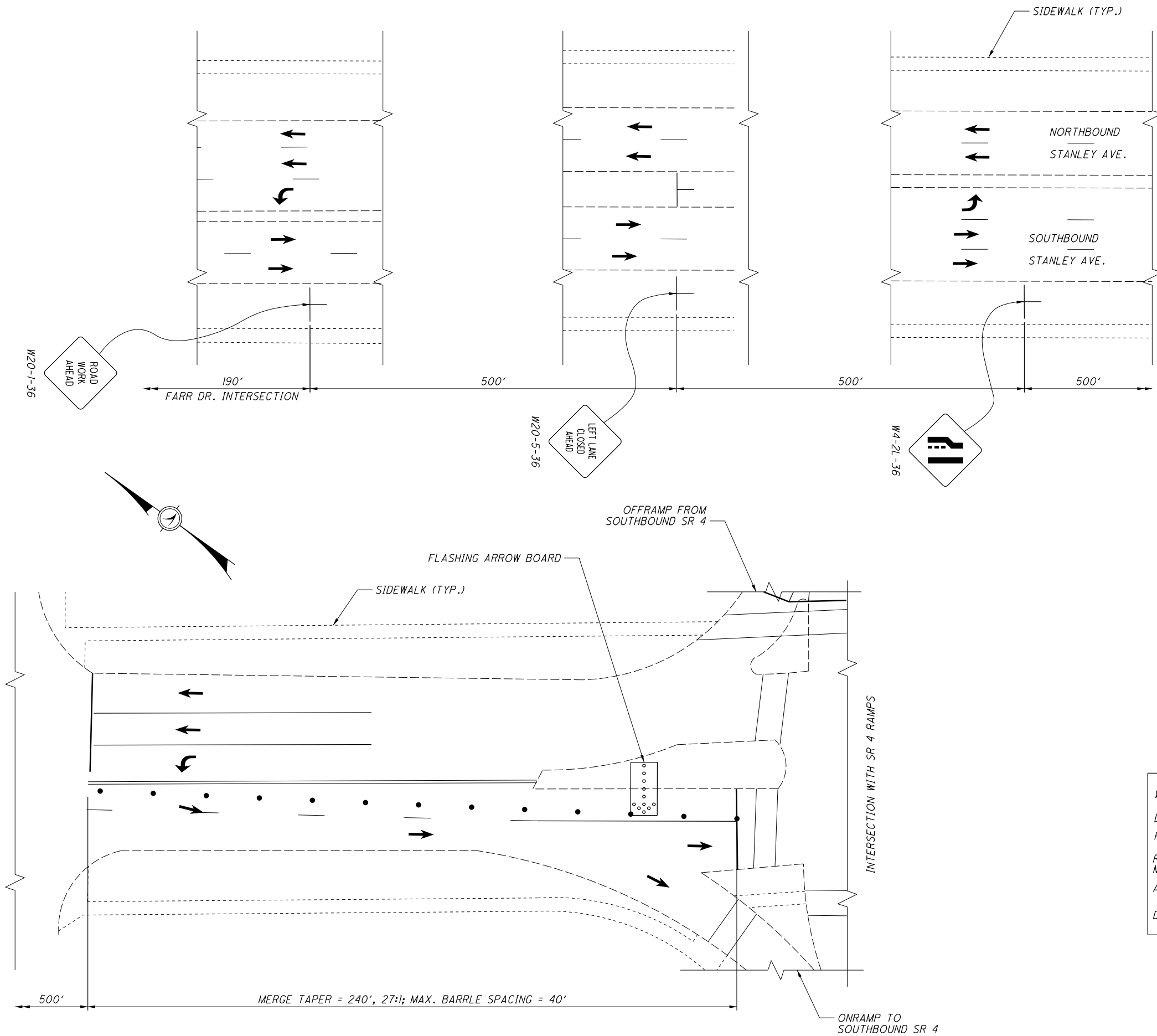


LEGEND

| | |
|--------------------------|--|
| WORK AREA | |
| DRUMS | |
| PORTABLE BARRIER (PB) | |
| REMOVE EXISTING MARKINGS | |
| ATTENUATOR | |
| DIRECTION OF TRAVEL | |
| TRUCK MOUNTED ATTENUATOR | |



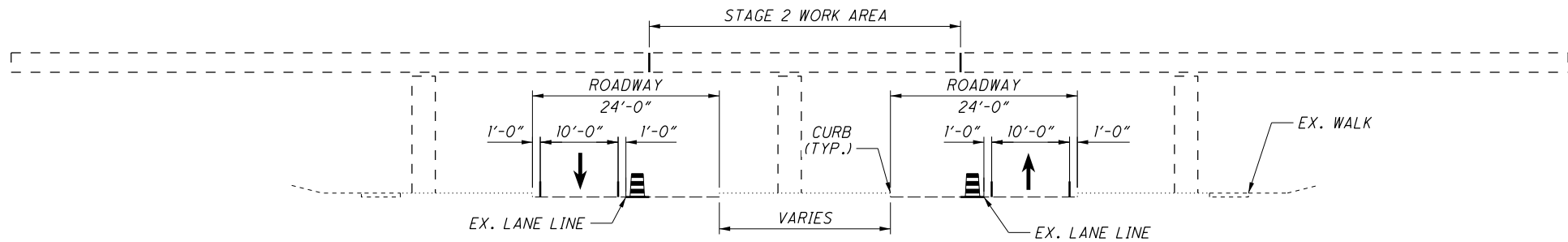
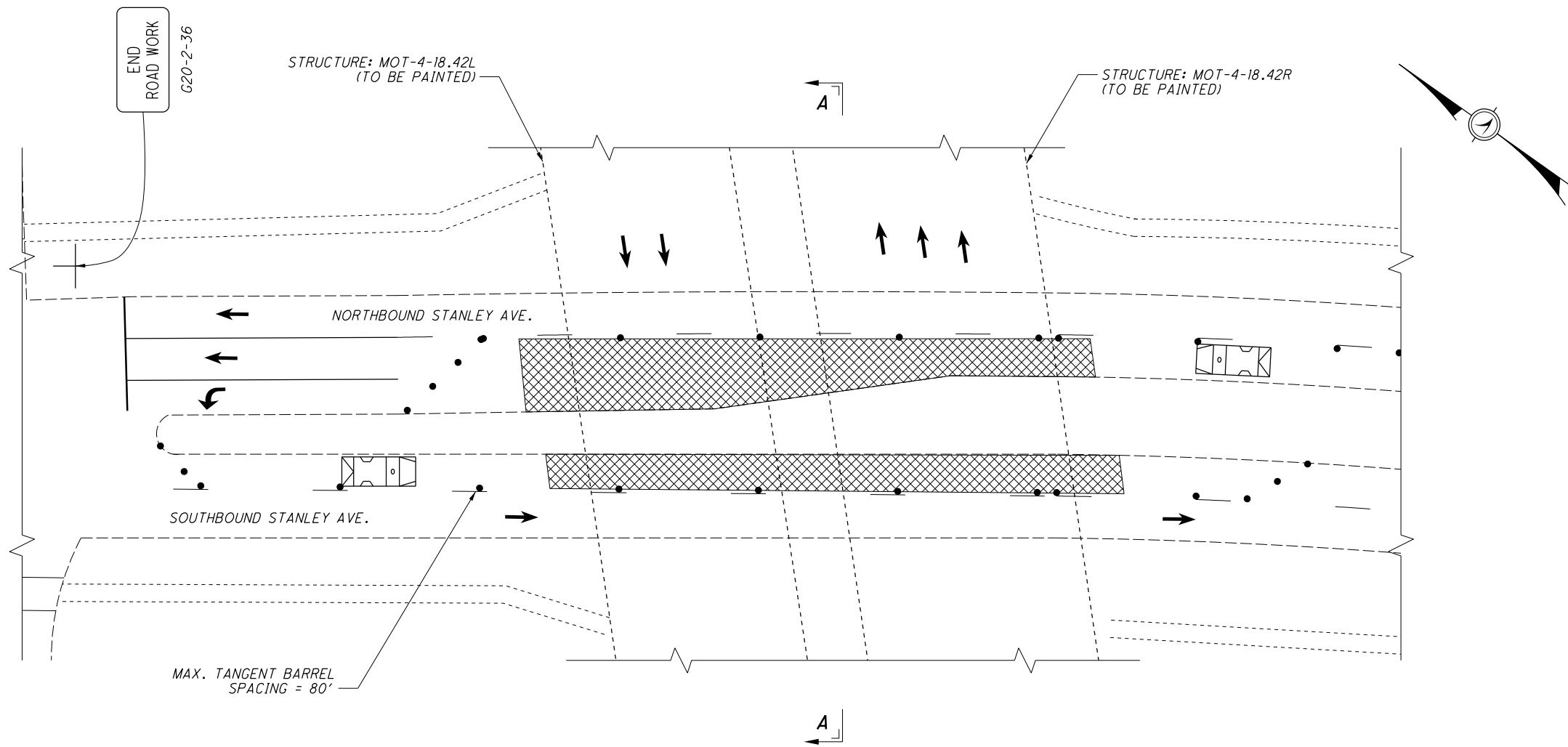
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LEGEND

| | |
|--------------------------|--|
| WORK AREA | |
| DRUMS | |
| PORTABLE BARRIER (PB) | |
| REMOVE EXISTING MARKINGS | |
| ATTENUATOR | |
| DIRECTION OF TRAVEL | |

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SECTION A-A STAGE 2
(BRIDGE ELEMENTS NOT SHOWN TO SCALE)

| LEGEND | |
|--------------------------|--|
| WORK AREA | |
| DRUMS | |
| PORTABLE BARRIER (PB) | |
| REMOVE EXISTING MARKINGS | |
| ATTENUATOR | |
| DIRECTION OF TRAVEL | |
| TRUCK-MOUNTED ATTENUATOR | |

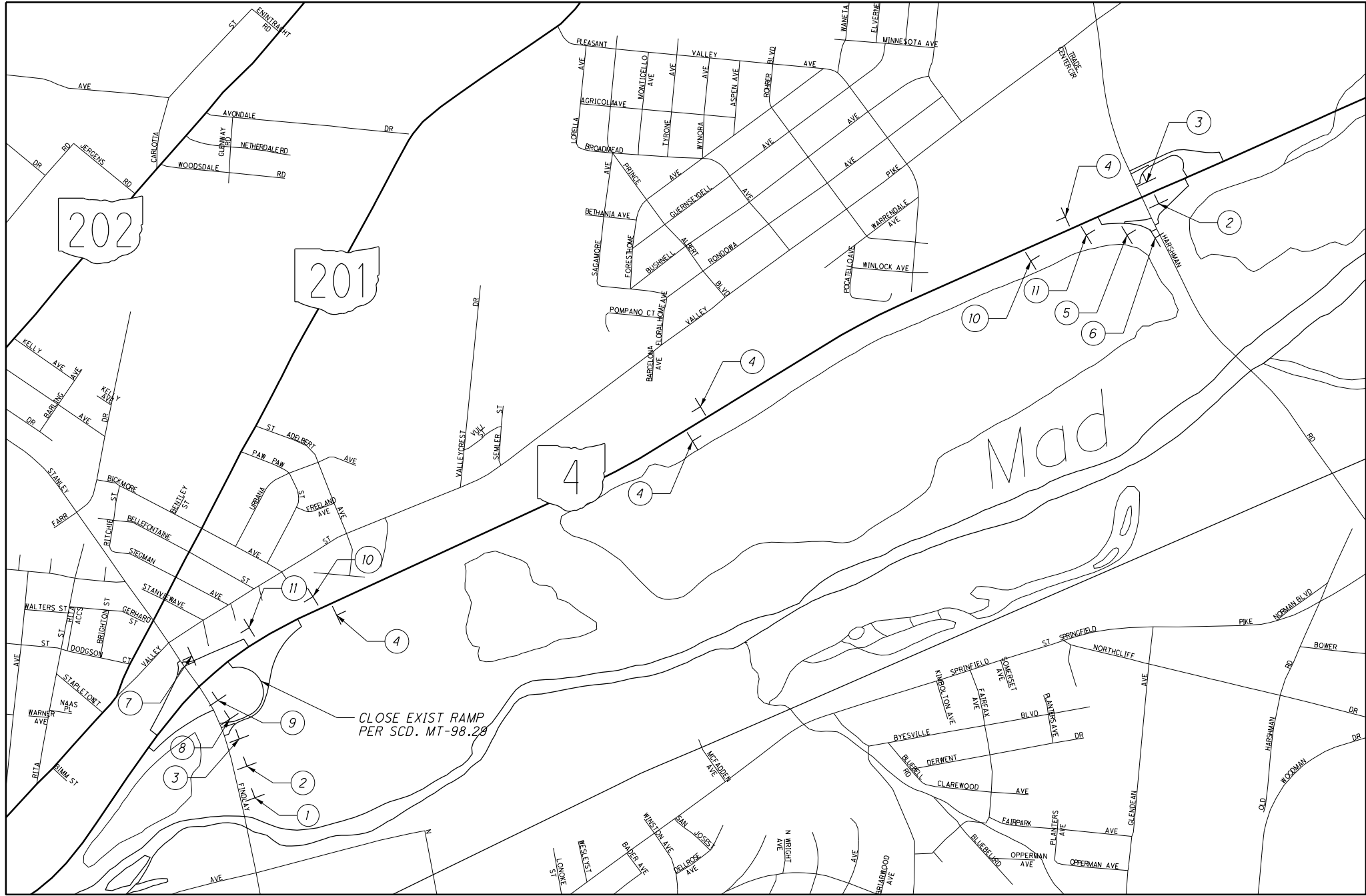
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MOT-4-18.42 L / R MAINTENANCE OF TRAFFIC
STAGE 2 (2 OF 3)

MOT-BP-FY18



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DETOUR MAP

7

END
DETOUR

M4-8a-24

8

ROAD CLOSED
LOCAL TRAFFIC ONLY

R11-3-66

DETOUR

M4-10R-48

9

ROAD
CLOSED

R11-2-48

SIGN LEGEND

1

DETOUR
AHEAD

W20-2-36

2

DETOUR

M4-8-24

NORTH

M3-1-24

STANLEY AVE

D3-1-60

DETOUR

M5-1-21

3

DETOUR

M4-8-24

NORTH

M3-1-24

STANLEY AVE

D3-1-60

DETOUR

M6-1-21

4

DETOUR

M4-8-24

NORTH

M3-1-24

STANLEY AVE

D3-1-60

DETOUR

M6-3-21

5

DETOUR

M4-8-24

NORTH

M3-1-24

STANLEY AVE

D3-1-60

DETOUR

M5-1-21

6

DETOUR

M4-8-24

NORTH

M3-1-24

STANLEY AVE

D3-1-60

DETOUR

M6-1-21

10

DETOUR

M4-8-24

NORTH

M3-1-24

STANLEY AVE

D3-1-60

DETOUR

M5-2-21

11

DETOUR

M4-8-24

NORTH

M3-1-24

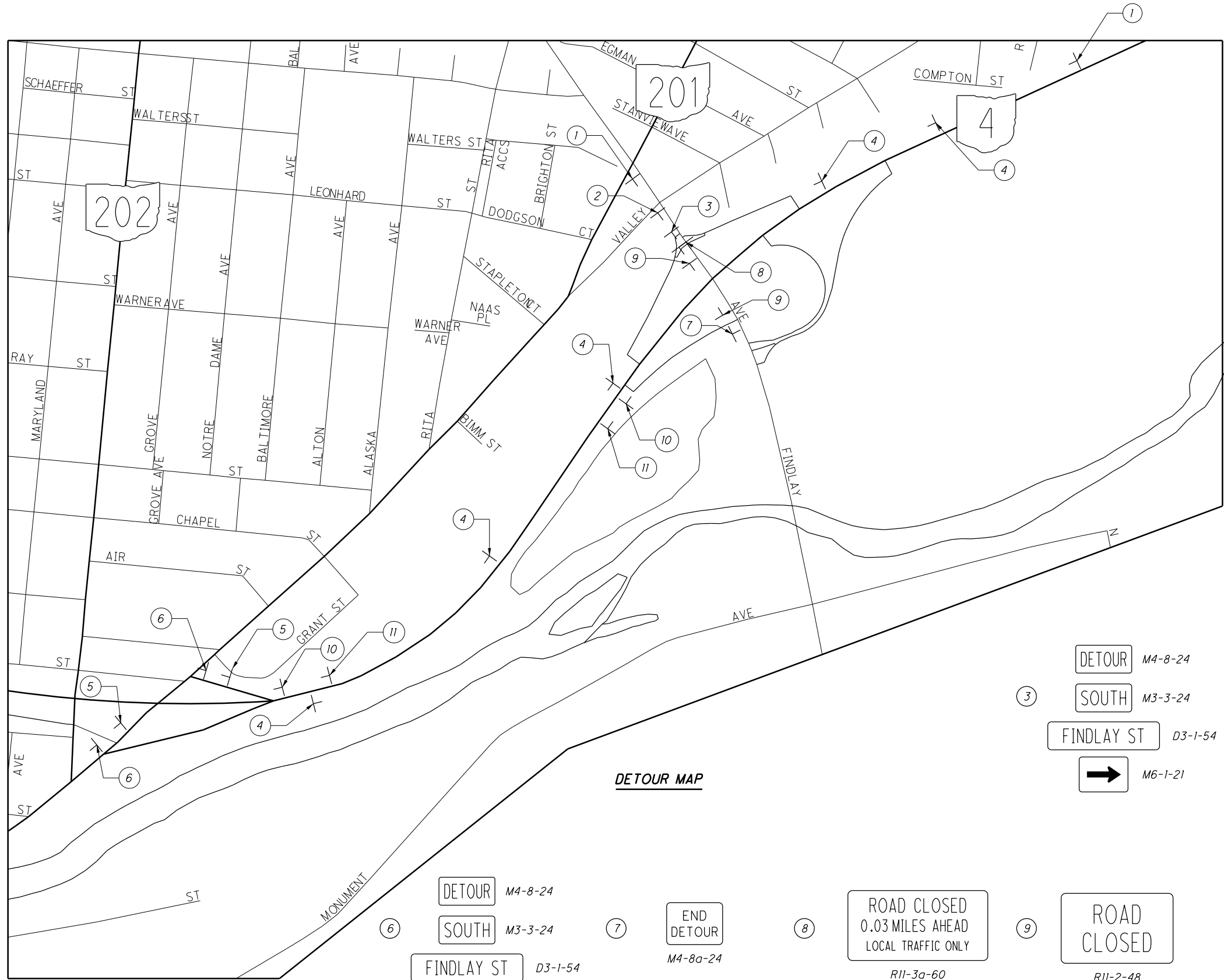
STANLEY AVE

D3-1-60

DETOUR

M6-2-21

I:\ProjectData\MOT\02303_BP_FY18\Design\Roadway\Sheets\02303_MD002.dgn Sheet 2/10/2017 8:21:47 AM cweiss



DETOUR MAP

SIGN LEGEND

1

DETOUR M4-8-24

SOUTH M3-3-24

FINDLAY ST D3-1-54

M6-1-21

2

END DETOUR M4-8a-24

3

ROAD CLOSED 0.03 MILES AHEAD LOCAL TRAFFIC ONLY R11-3a-60

DETOUR M4-10R-48

4

ROAD CLOSED R11-2-48

5

DETOUR M4-8-24

SOUTH M3-3-24

FINDLAY ST D3-1-54

M6-2-21

6

DETOUR M4-8-24

SOUTH M3-3-24

FINDLAY ST D3-1-54

M5-2-21

7

DETOUR M4-8-24

SOUTH M3-3-24

FINDLAY ST D3-1-54

M6-1-21

8

DETOUR M4-8-24

SOUTH M3-3-24

FINDLAY ST D3-1-54

M6-3-21

9

DETOUR M4-8-24

SOUTH M3-3-24

FINDLAY ST D3-1-54

M5-1-21

10

DETOUR AHEAD W20-2-36

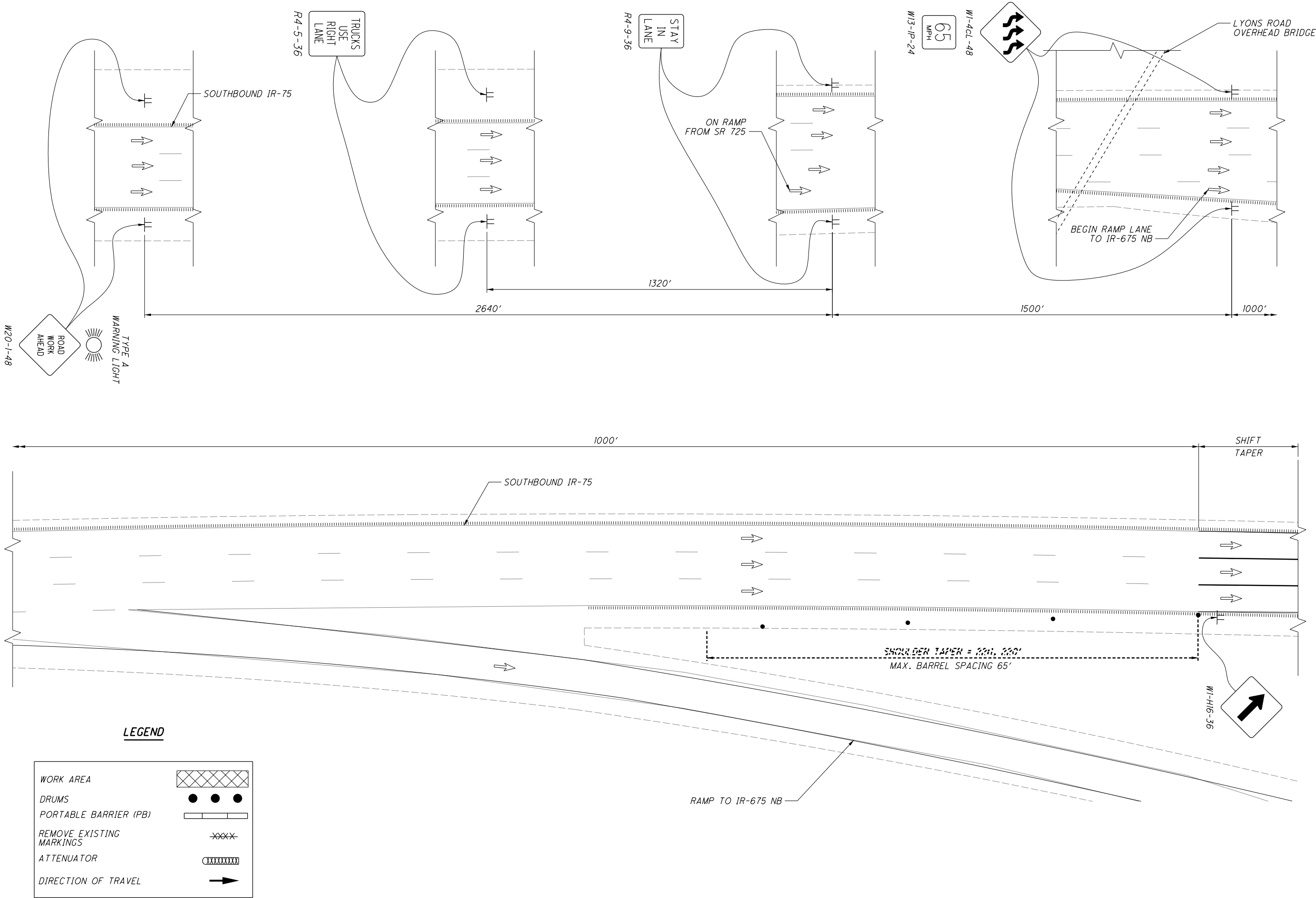
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DETOUR M4-8-24

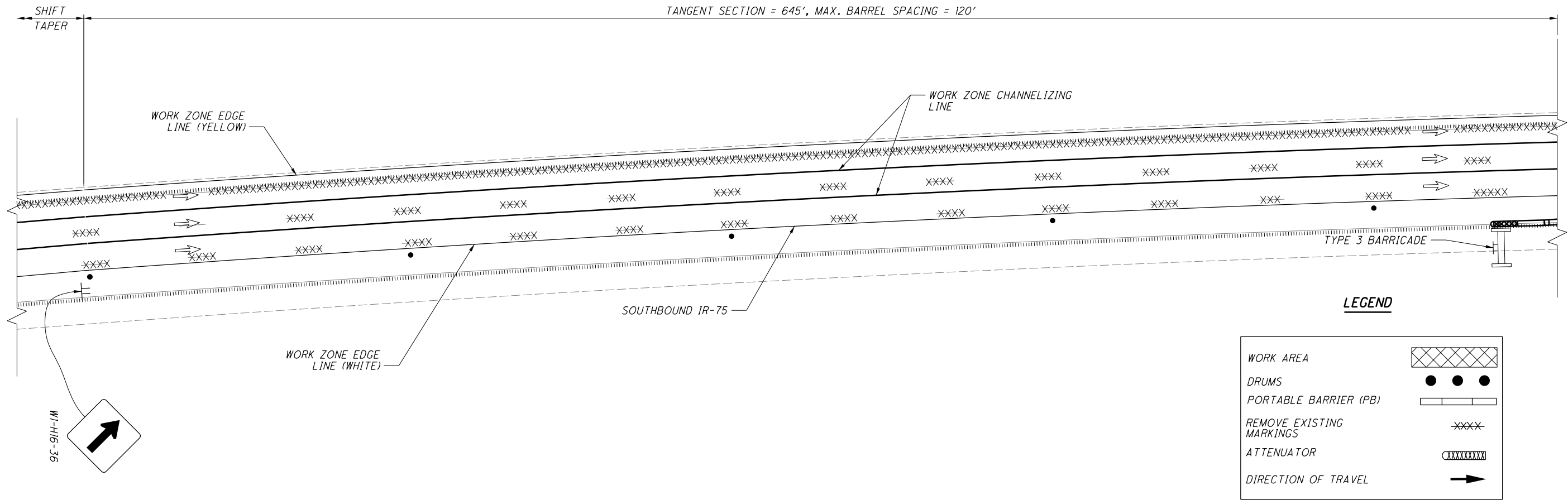
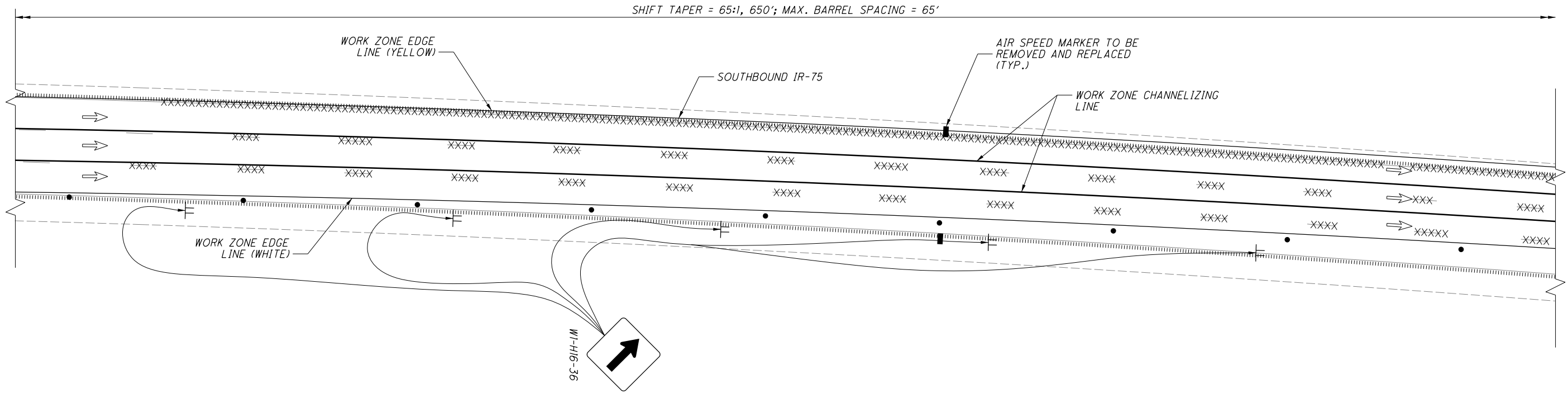
SOUTH M3-3-24

FINDLAY ST D3-1-54

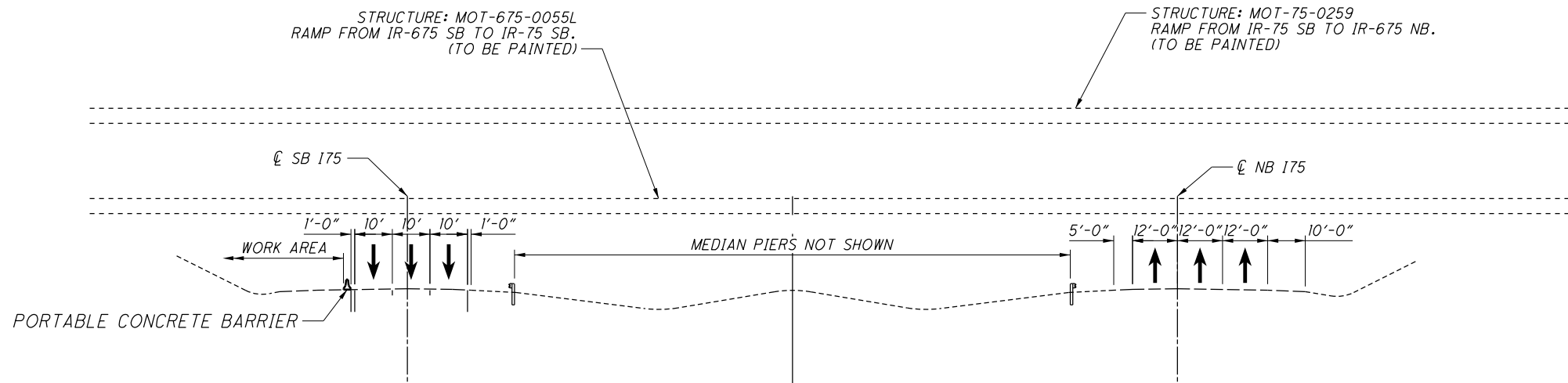
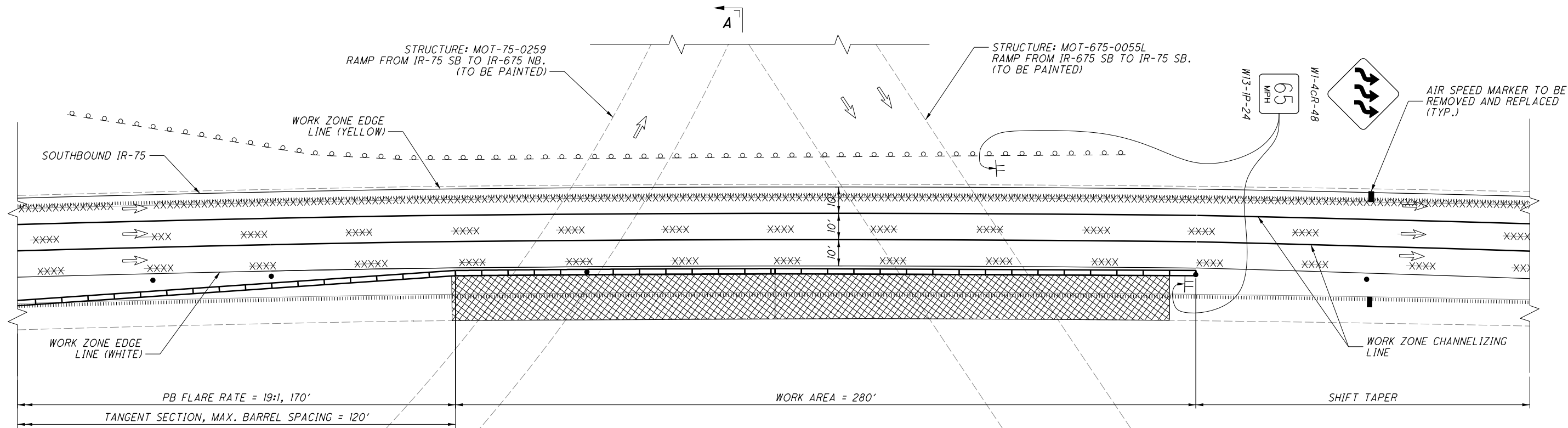
M5-1-21



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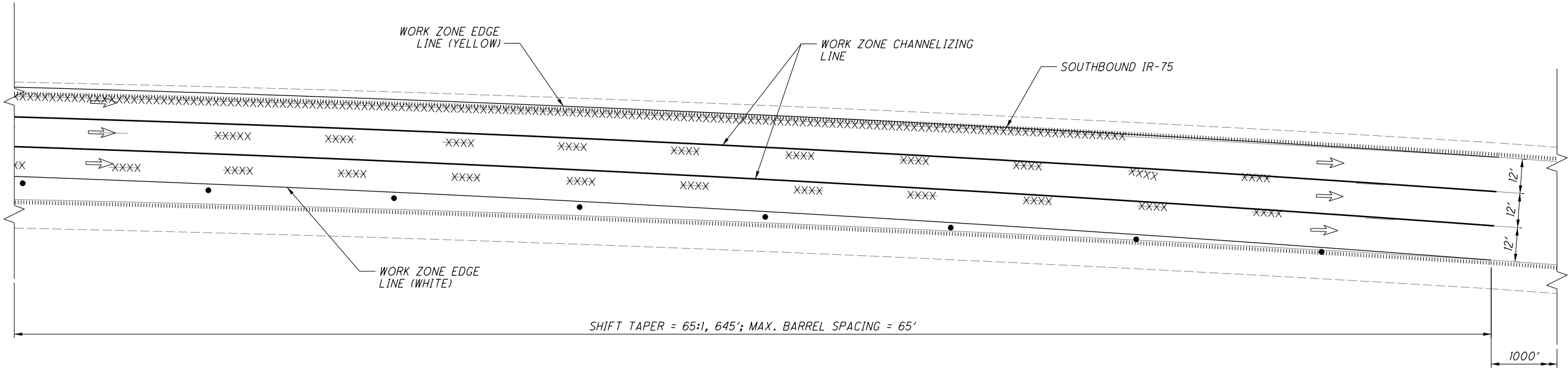
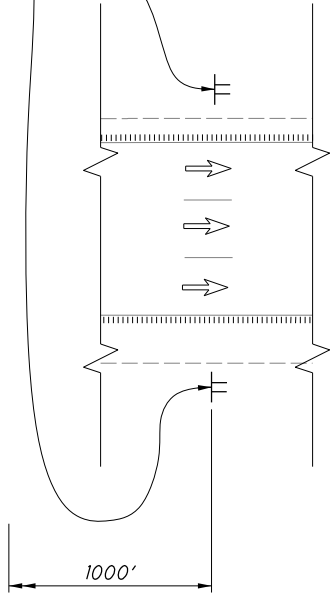
SECTION A-A
(BRIDGE ELEMENTS NOT DRAWN TO SCALE)

LEGEND

| | |
|--------------------------|--|
| WORK AREA | |
| DRUMS | |
| PORTABLE BARRIER (PB) | |
| REMOVE EXISTING MARKINGS | |
| ATTENUATOR | |
| DIRECTION OF TRAVEL | |

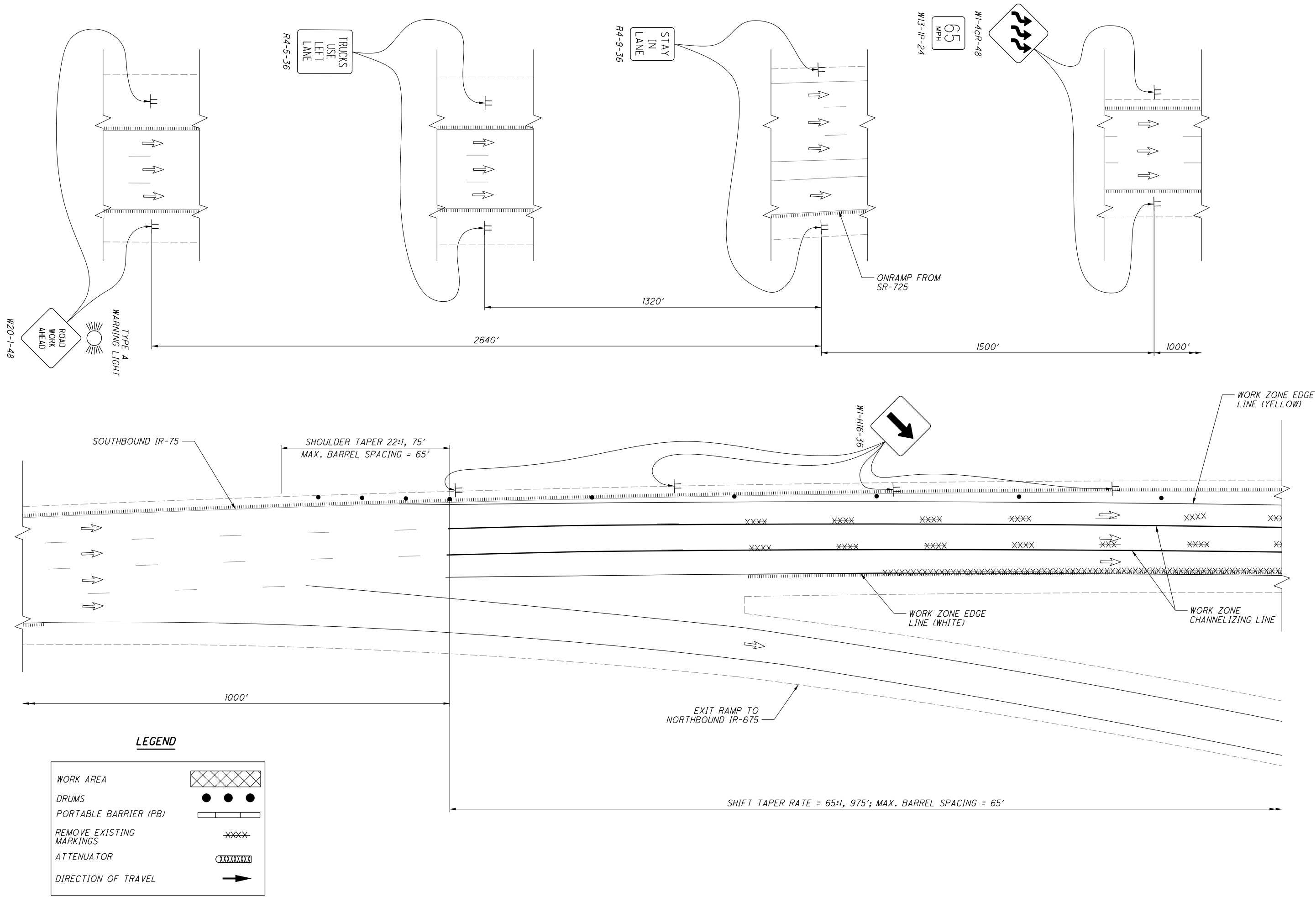
I:\ProjectData\MOT\02303_BP_FY18\Design\Roadway\Sheets\02303_MH003.dgn Sheet 2/10/2017 8:28:25 AM cweiss

END
ROAD WORK
G20-2-48

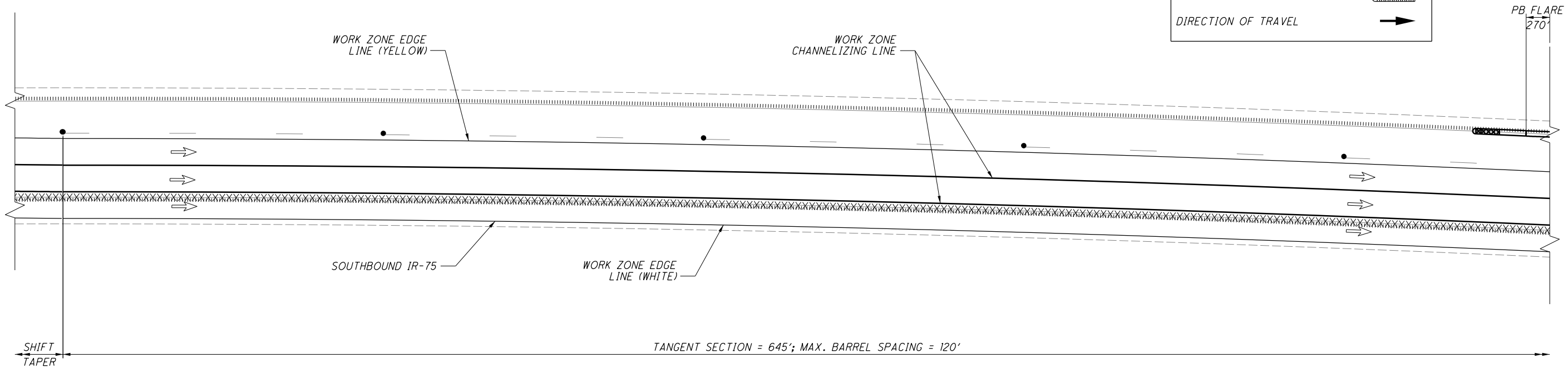
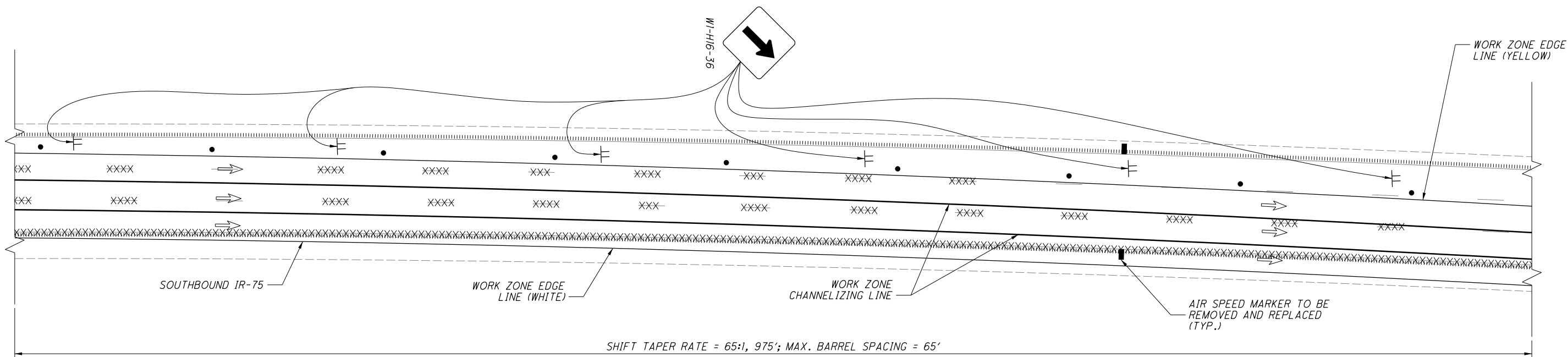


LEGEND

| | |
|--------------------------|--|
| WORK AREA | |
| DRUMS | |
| PORTABLE BARRIER (PB) | |
| REMOVE EXISTING MARKINGS | |
| ATTENUATOR | |
| DIRECTION OF TRAVEL | |



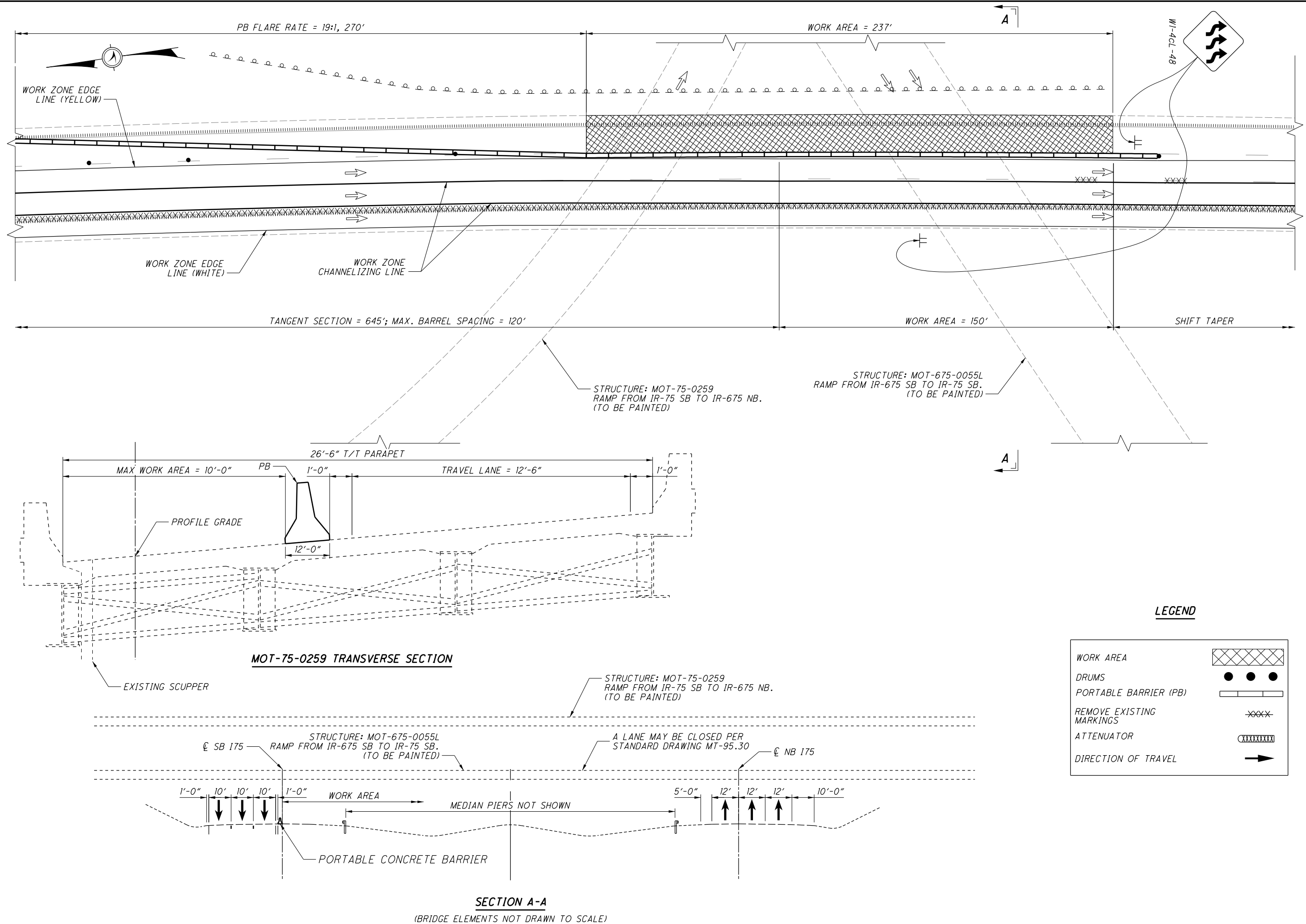
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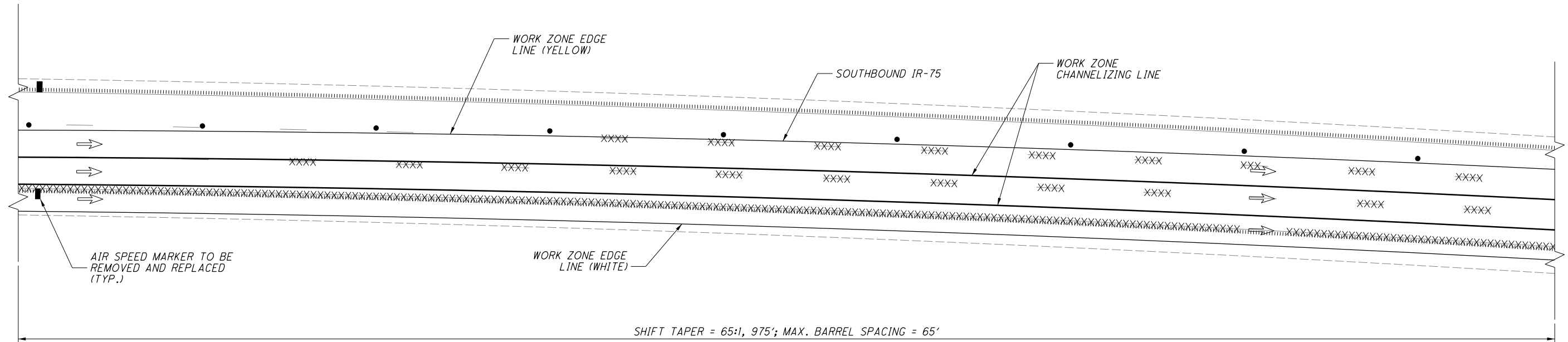
LEGEND

| | |
|--------------------------|--|
| WORK AREA | |
| DRUMS | |
| PORTABLE BARRIER (PB) | |
| REMOVE EXISTING MARKINGS | |
| ATTENUATOR | |
| DIRECTION OF TRAVEL | |

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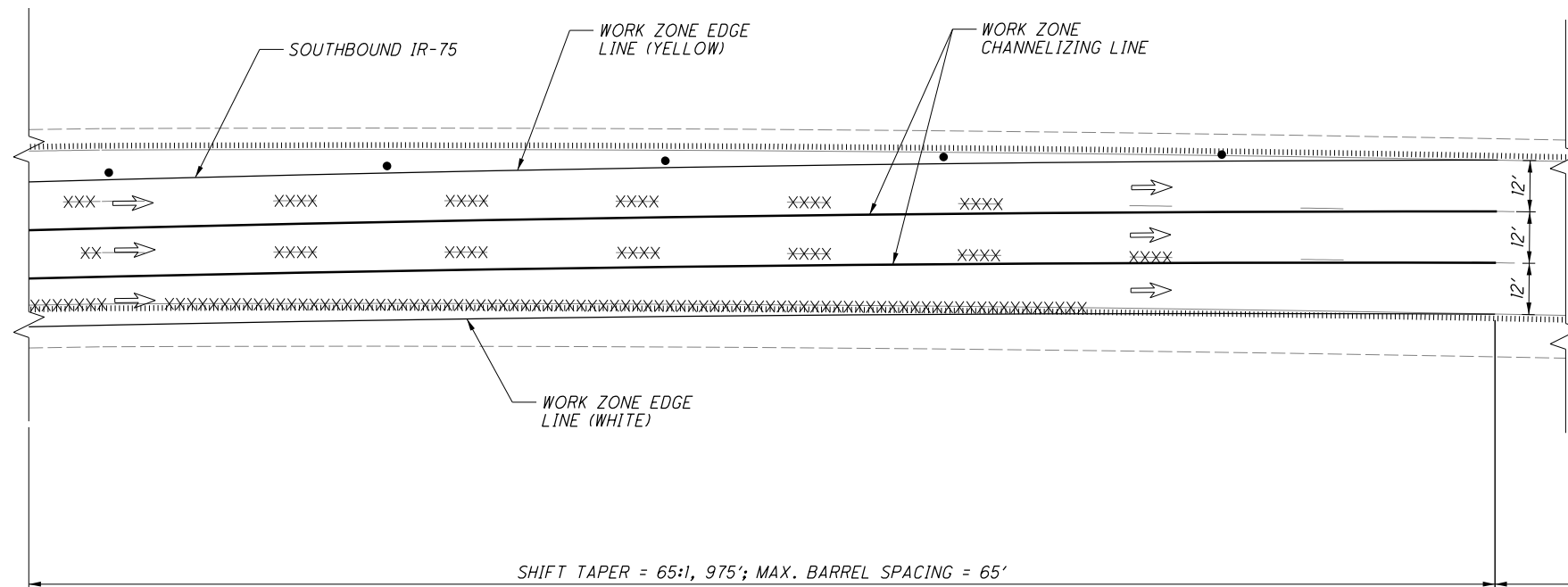


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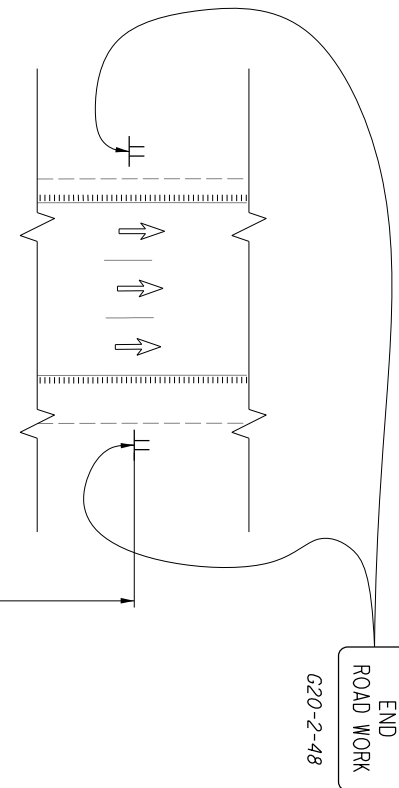


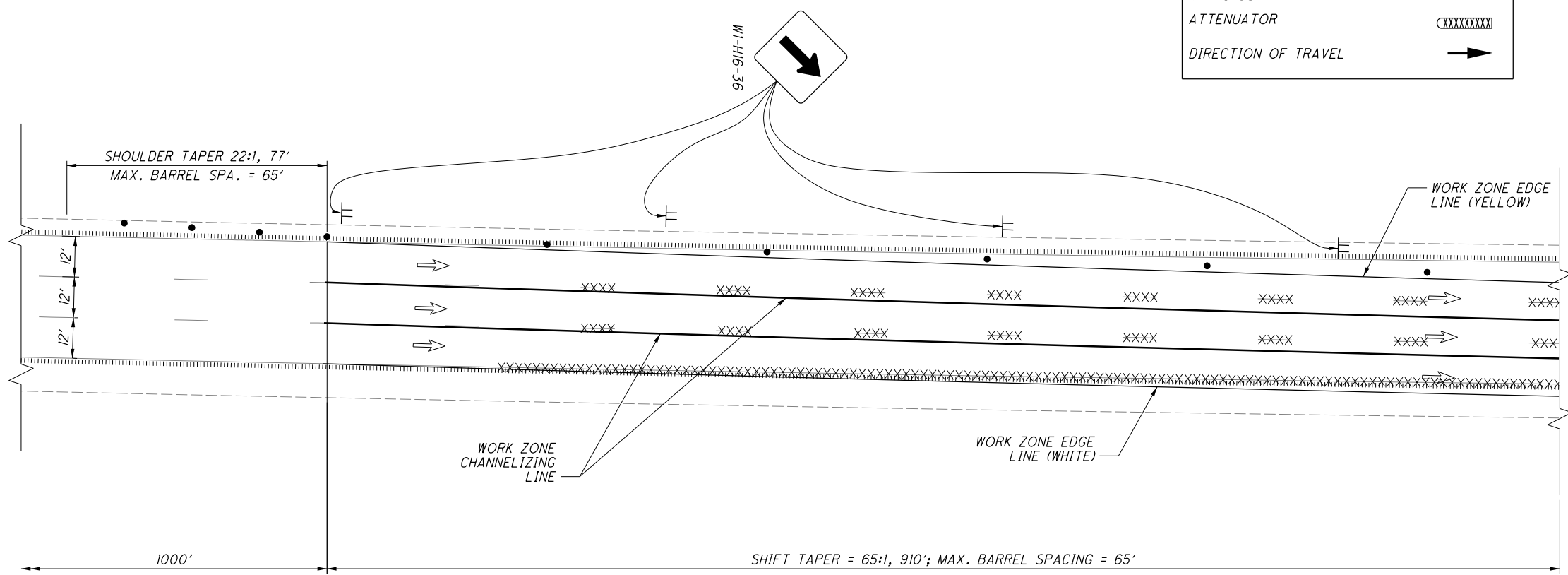
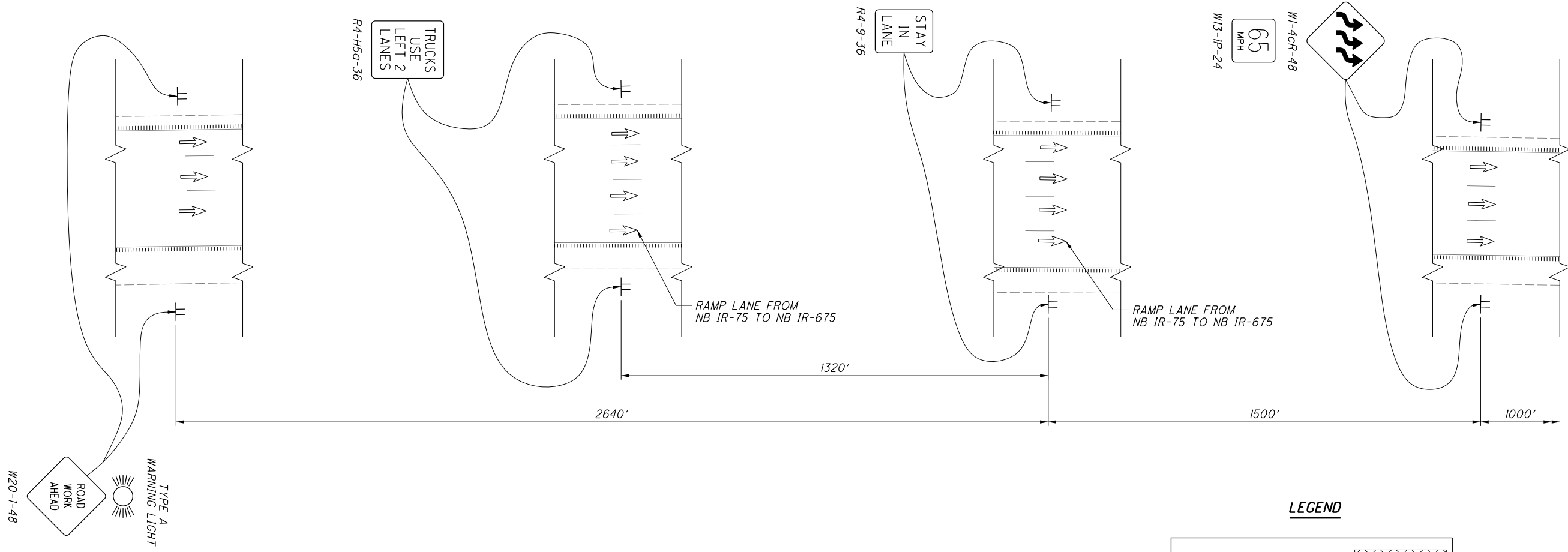
LEGEND

| | |
|--------------------------|--|
| WORK AREA | |
| DRUMS | |
| PORTABLE BARRIER (PB) | |
| REMOVE EXISTING MARKINGS | |
| ATTENUATOR | |
| DIRECTION OF TRAVEL | |



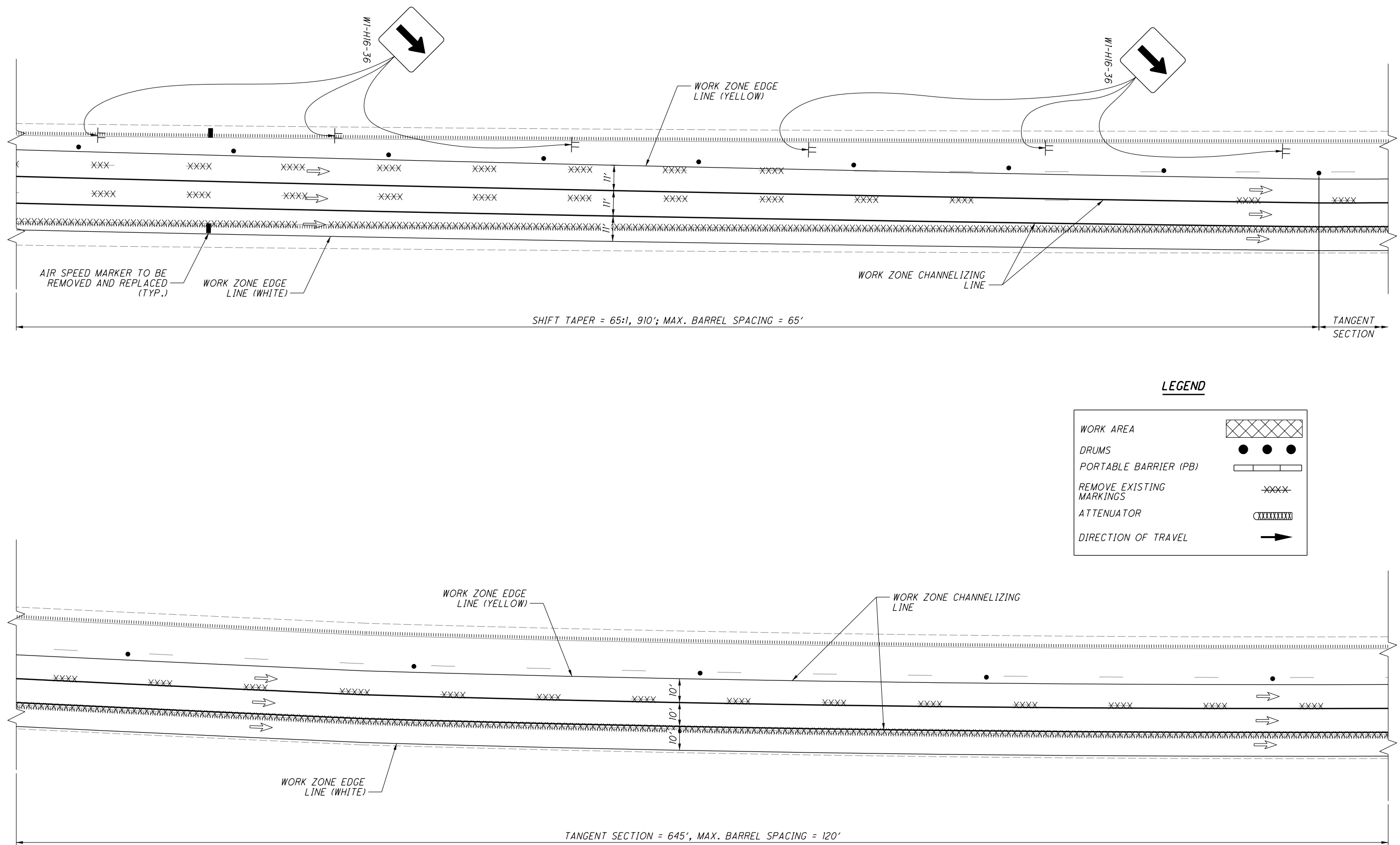
1000'



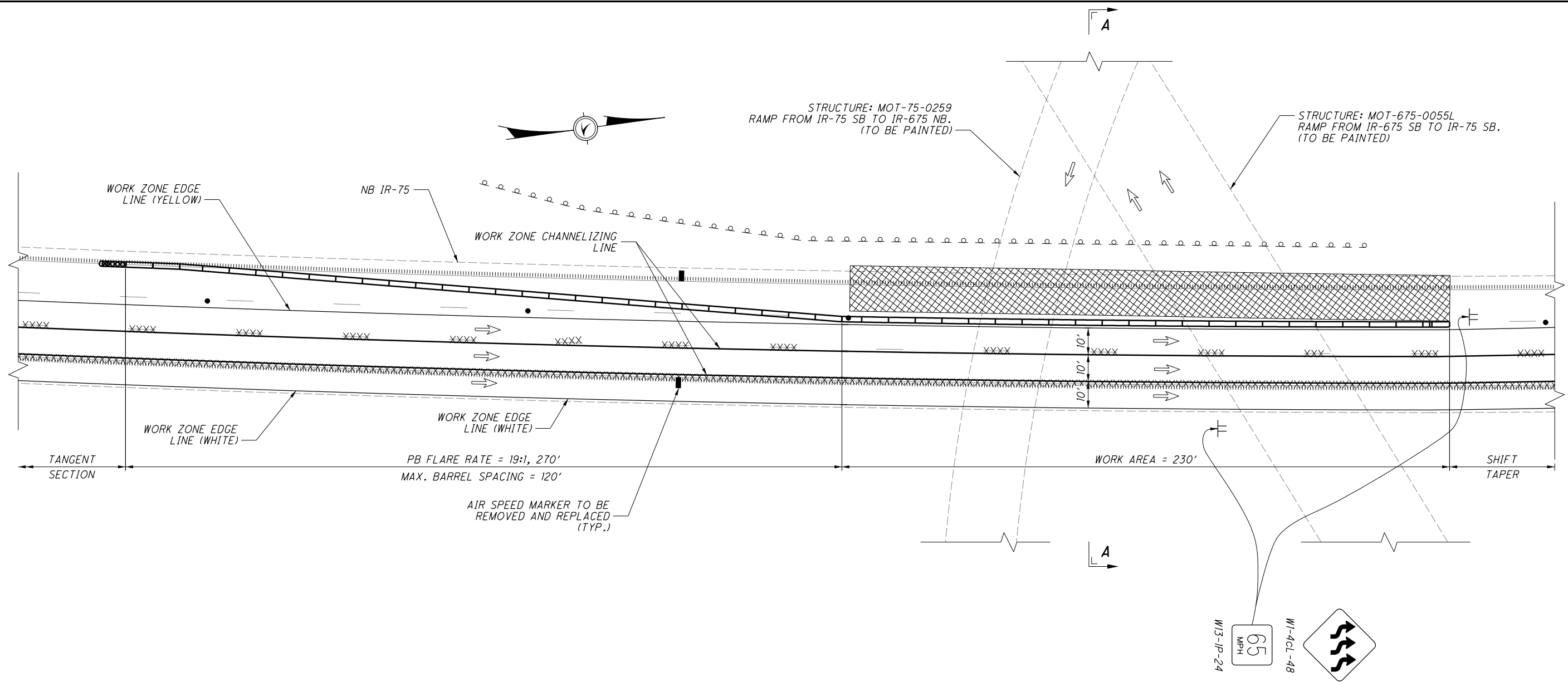


LEGEND

| | |
|--------------------------|--|
| WORK AREA | |
| DRUMS | |
| PORTABLE BARRIER (PB) | |
| REMOVE EXISTING MARKINGS | |
| ATTENUATOR | |
| DIRECTION OF TRAVEL | |

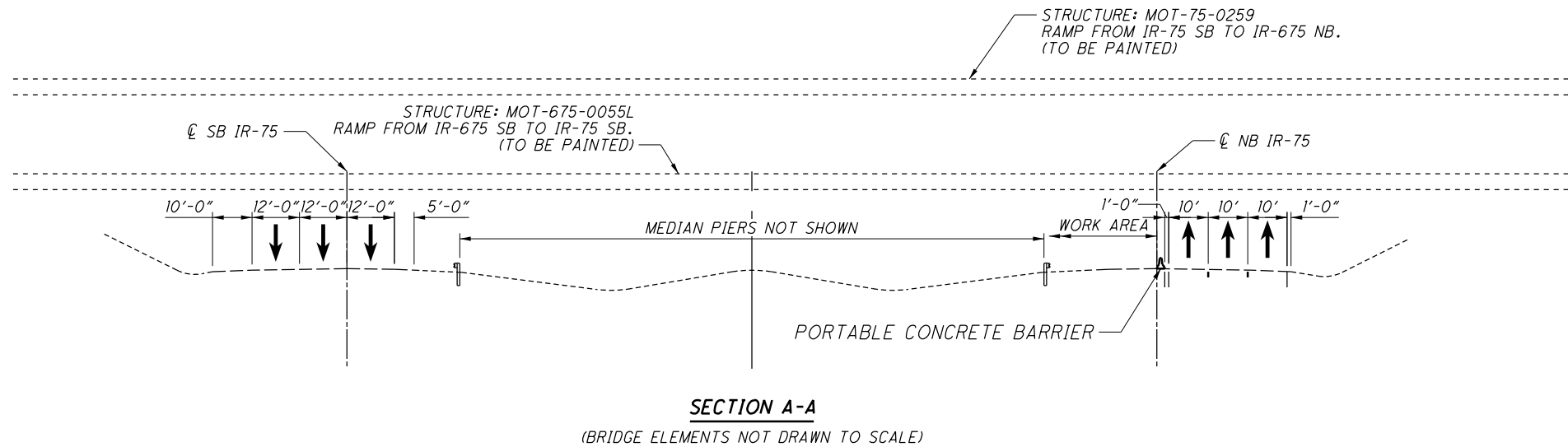


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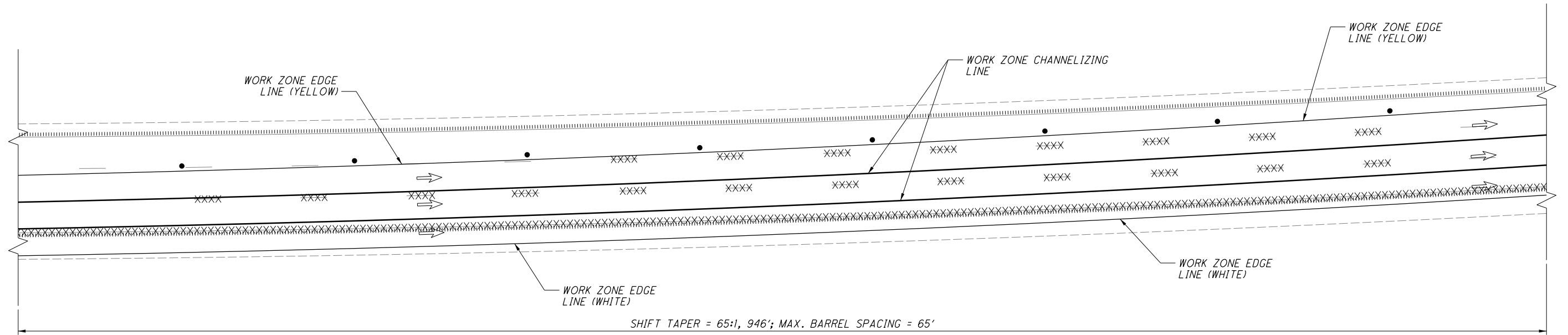


LEGEND

| | |
|--------------------------|--|
| WORK AREA | |
| DRUMS | |
| PORTABLE BARRIER (PB) | |
| REMOVE EXISTING MARKINGS | |
| ATTENUATOR | |
| DIRECTION OF TRAVEL | |

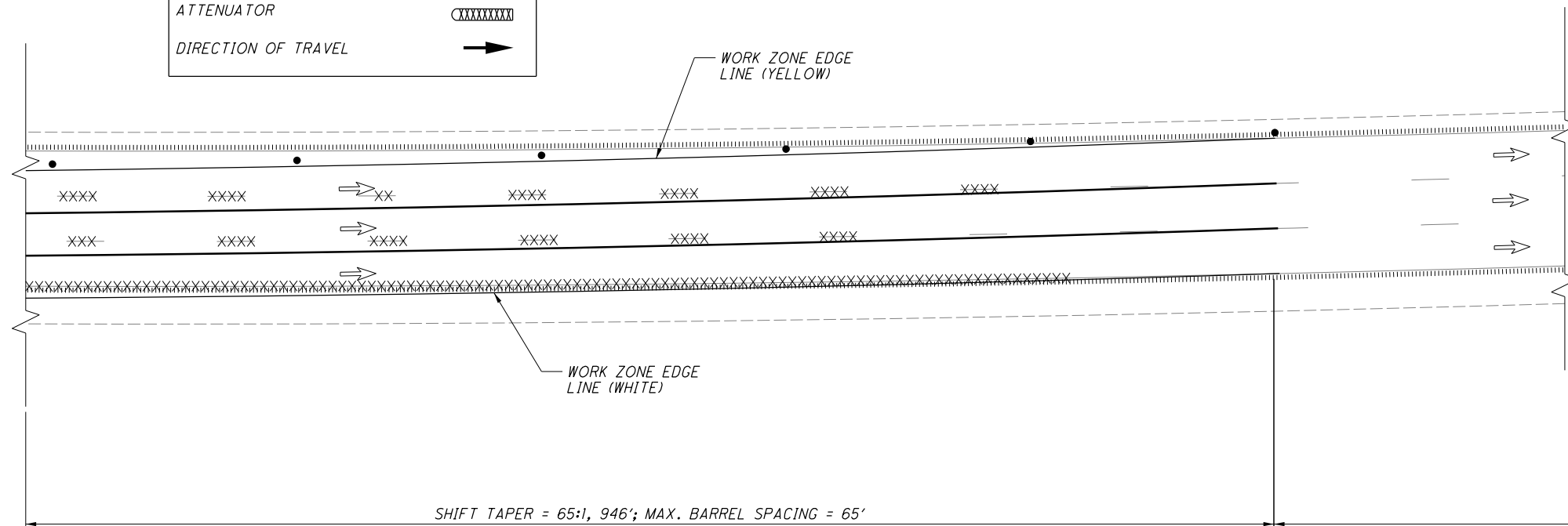


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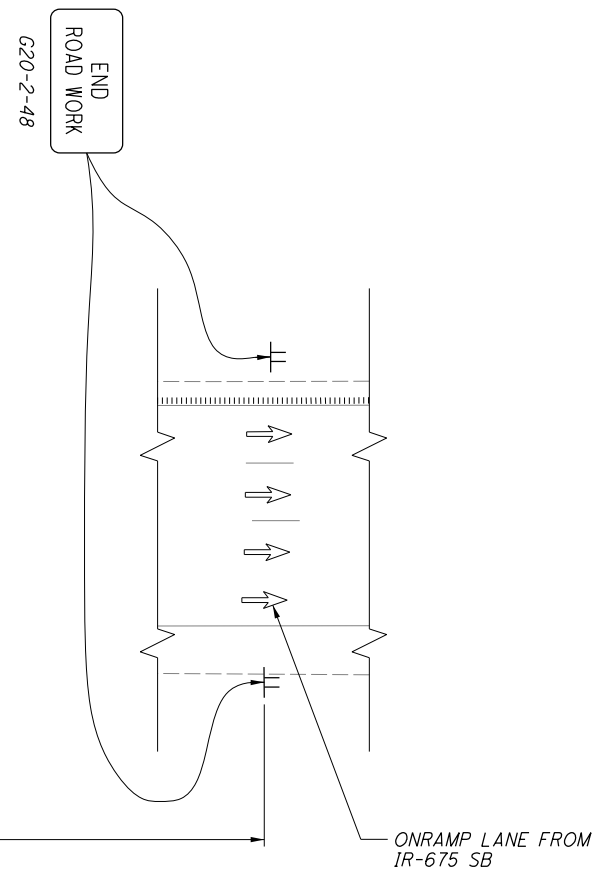


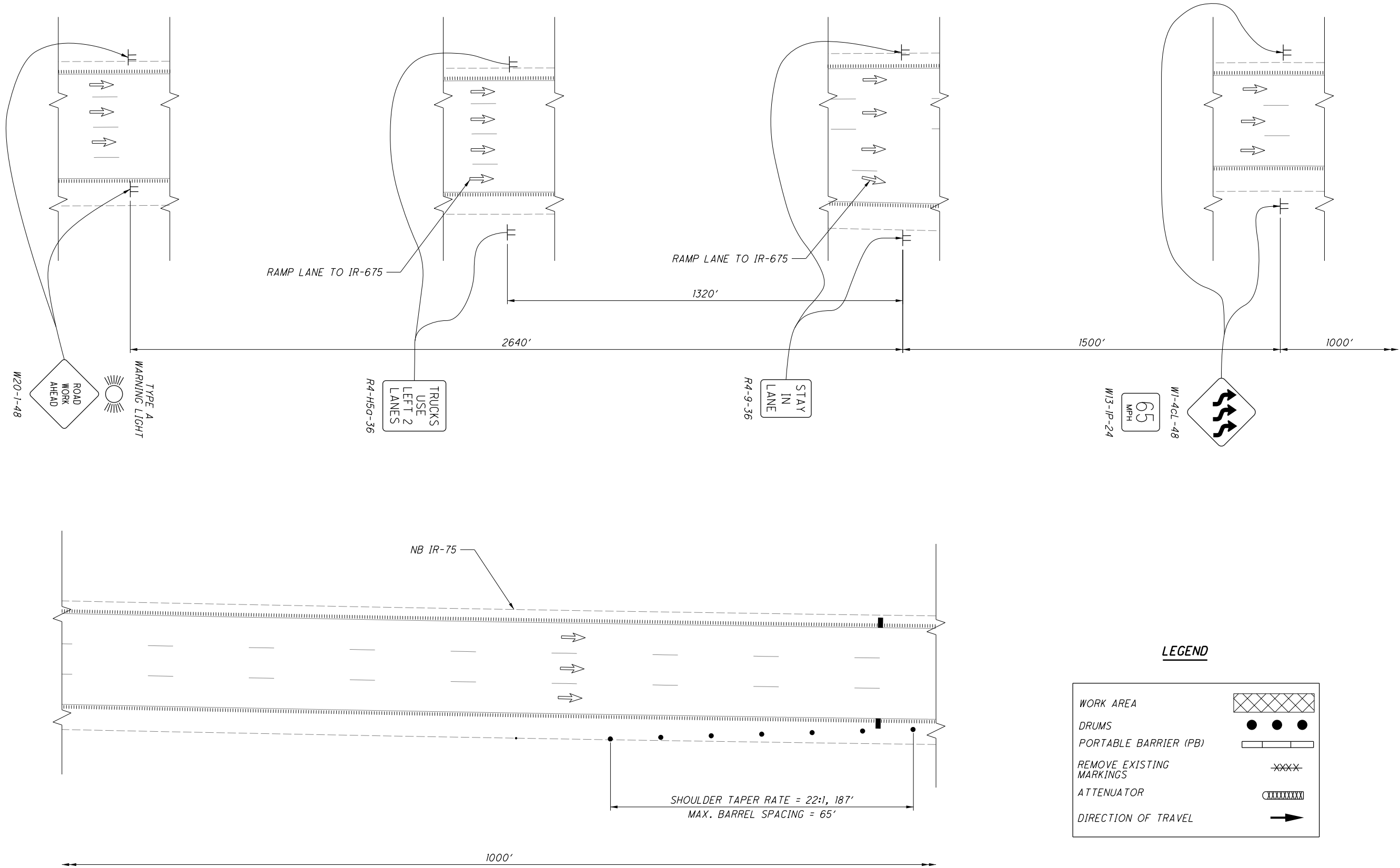
LEGEND

| | |
|--------------------------|--|
| WORK AREA | |
| DRUMS | |
| PORTABLE BARRIER (PB) | |
| REMOVE EXISTING MARKINGS | |
| ATTENUATOR | |
| DIRECTION OF TRAVEL | |

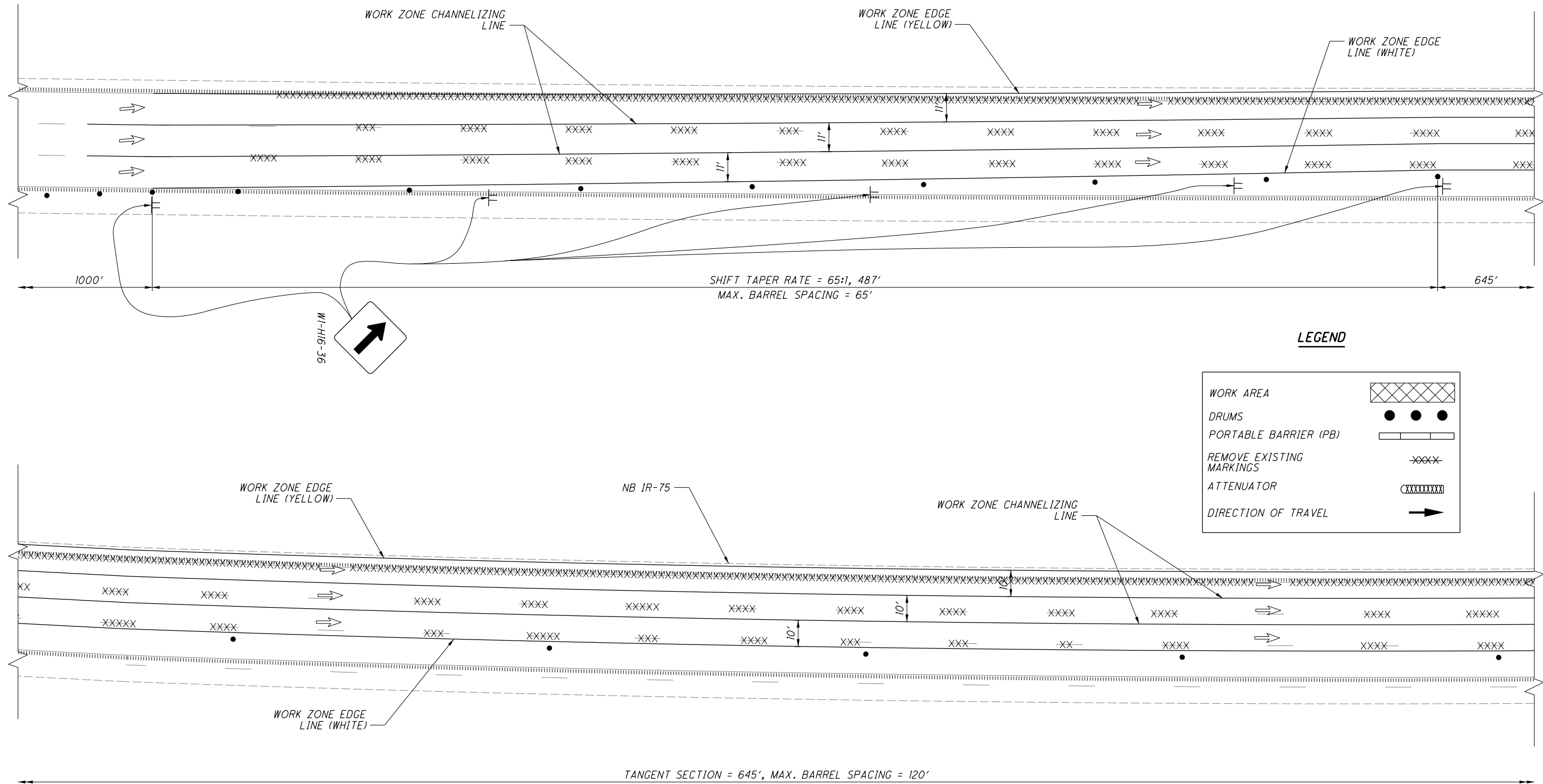


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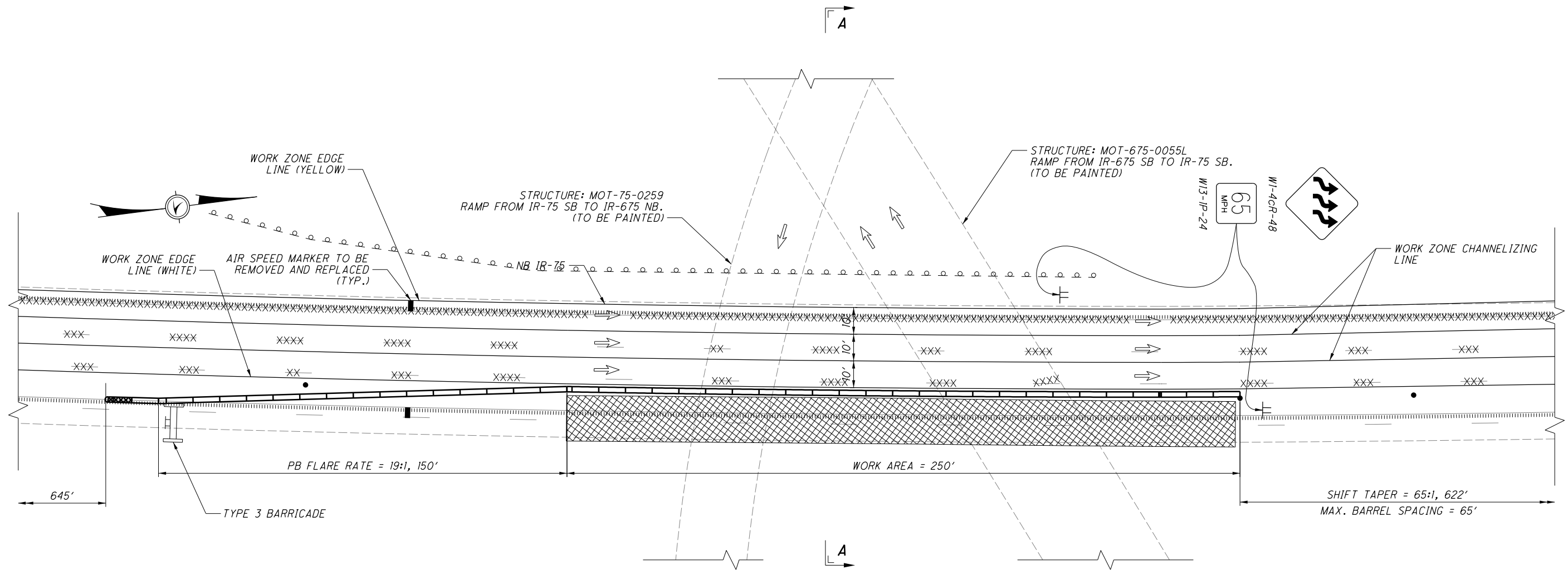




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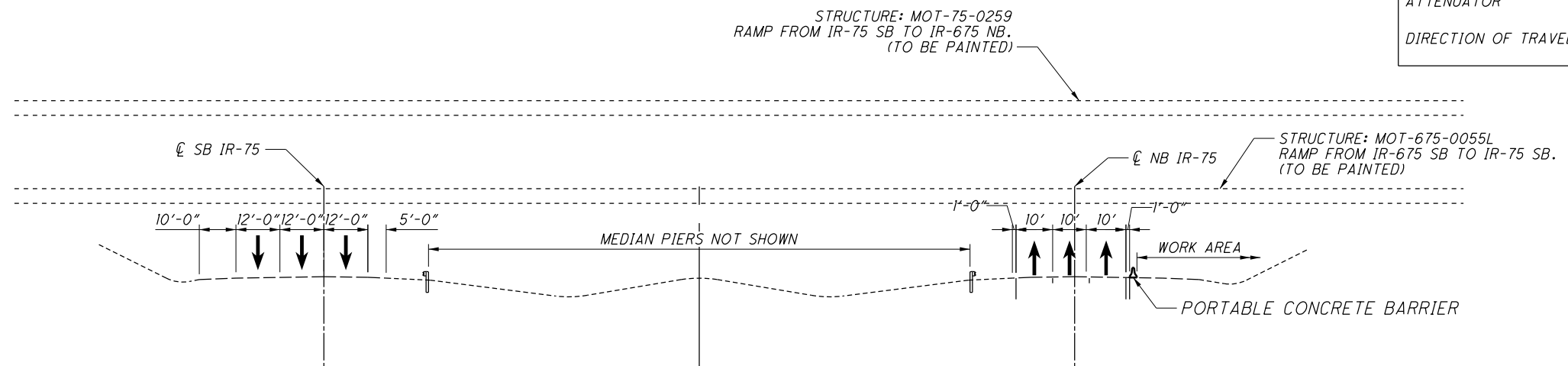
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NOTES & LEGEND

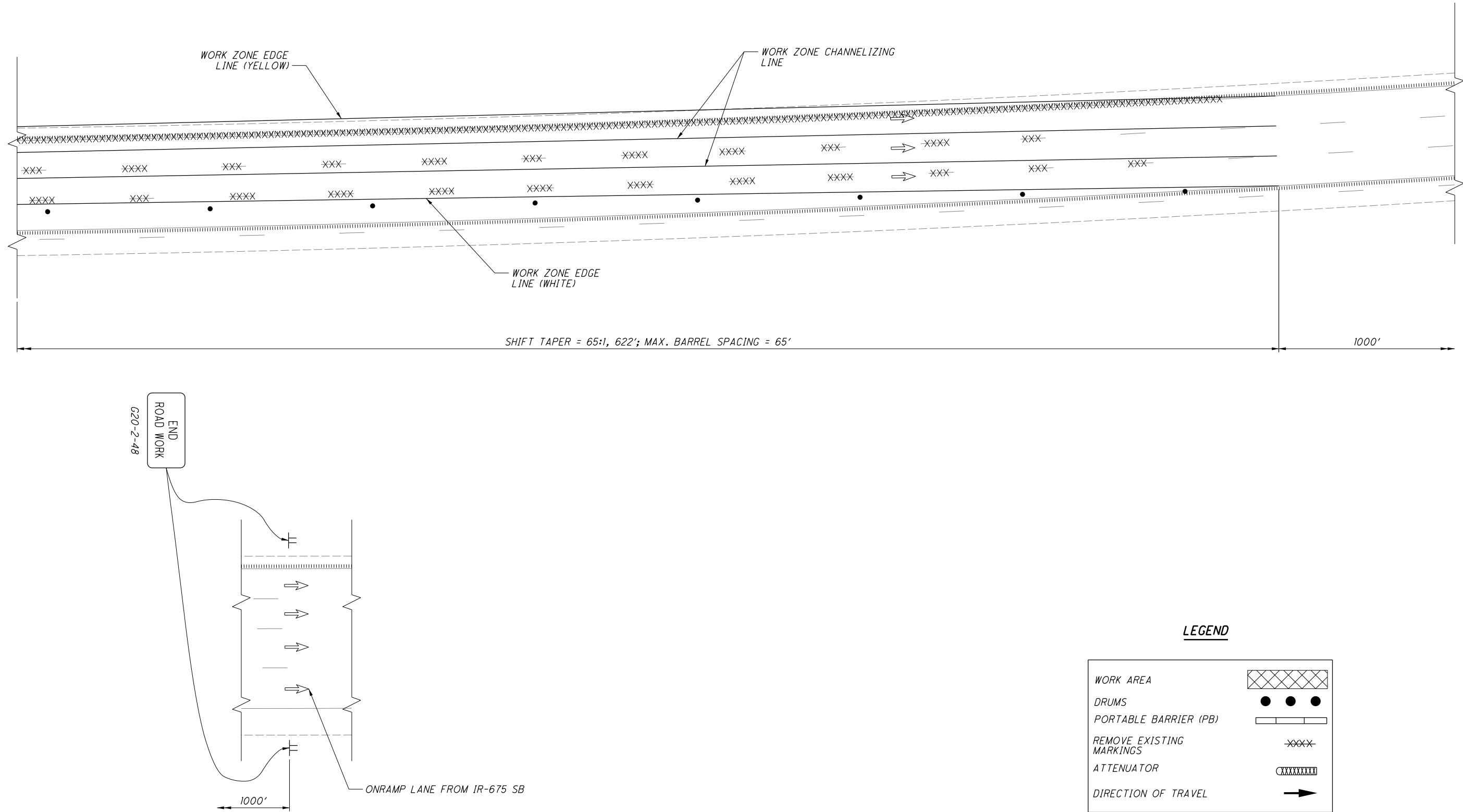
NB IR-75 MINIMUM VERTICAL CLEARANCE = 16'-6"

| | |
|--------------------------|--|
| WORK AREA | |
| DRUMS | |
| PORTABLE BARRIER (PB) | |
| REMOVE EXISTING MARKINGS | |
| ATTENUATOR | |
| DIRECTION OF TRAVEL | |



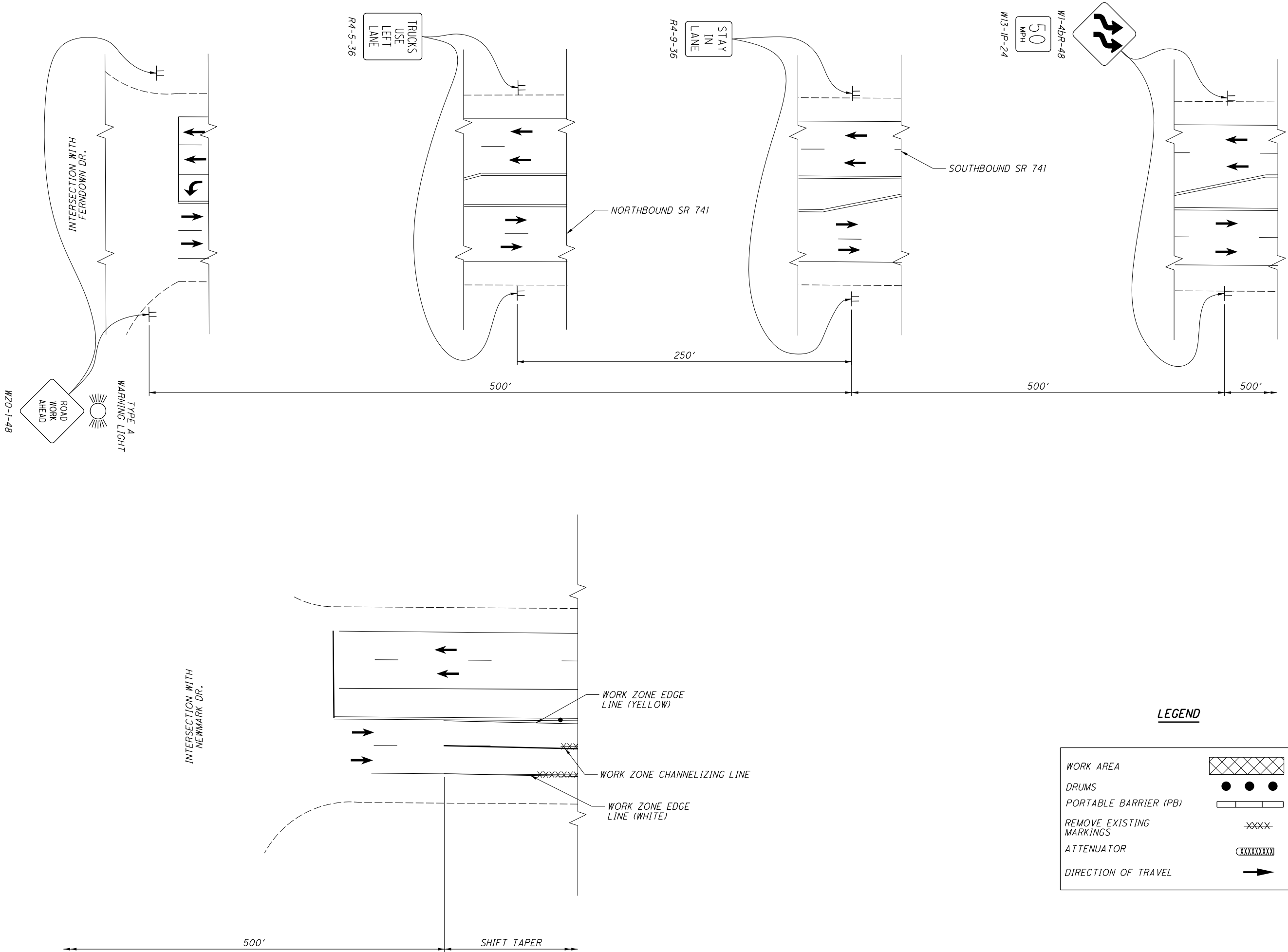
SECTION A-A
(BRIDGE ELEMENTS NOT DRAWN TO SCALE)

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LEGEND

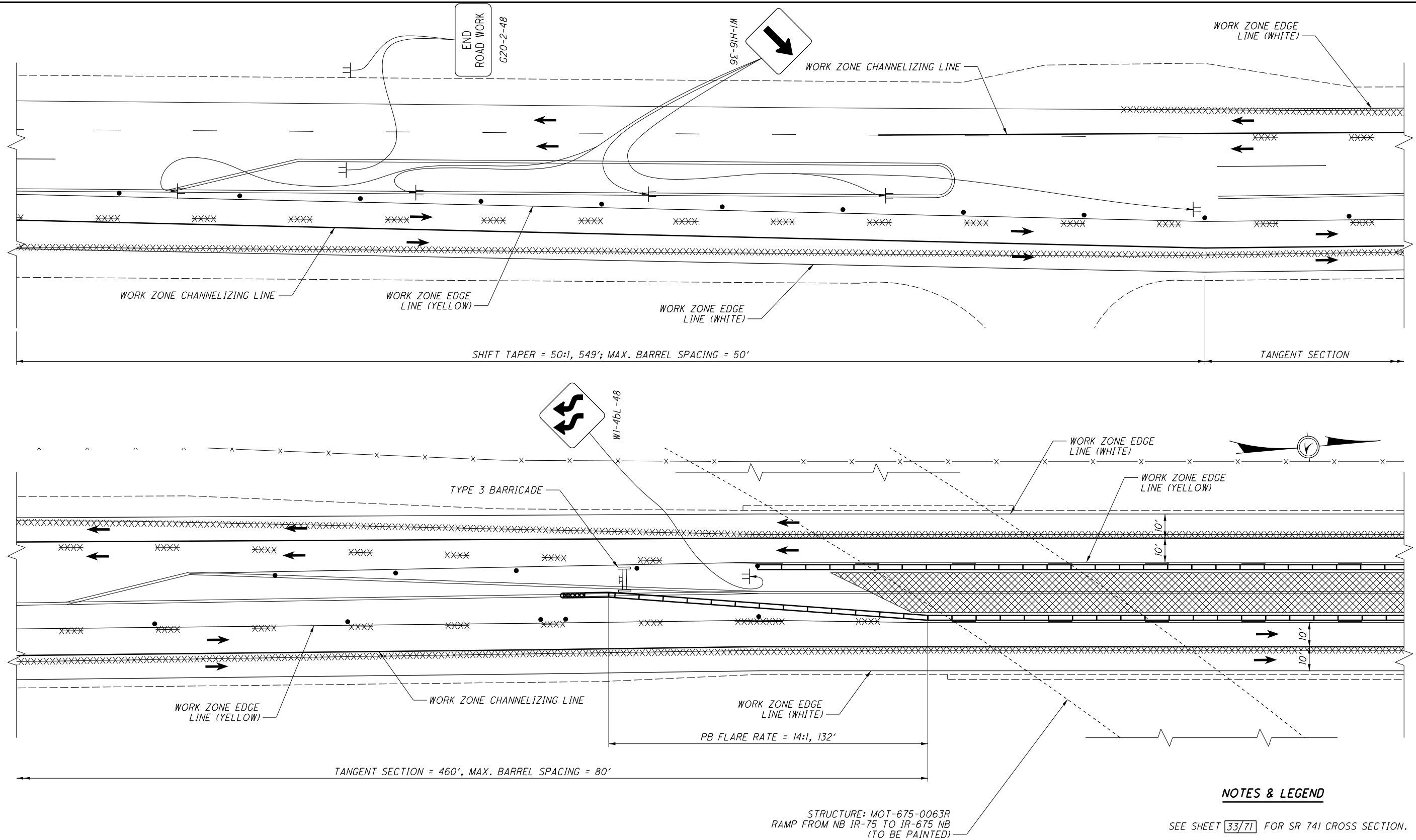
| | |
|--------------------------|--|
| WORK AREA | |
| DRUMS | |
| PORTABLE BARRIER (PB) | |
| REMOVE EXISTING MARKINGS | |
| ATTENUATOR | |
| DIRECTION OF TRAVEL | |



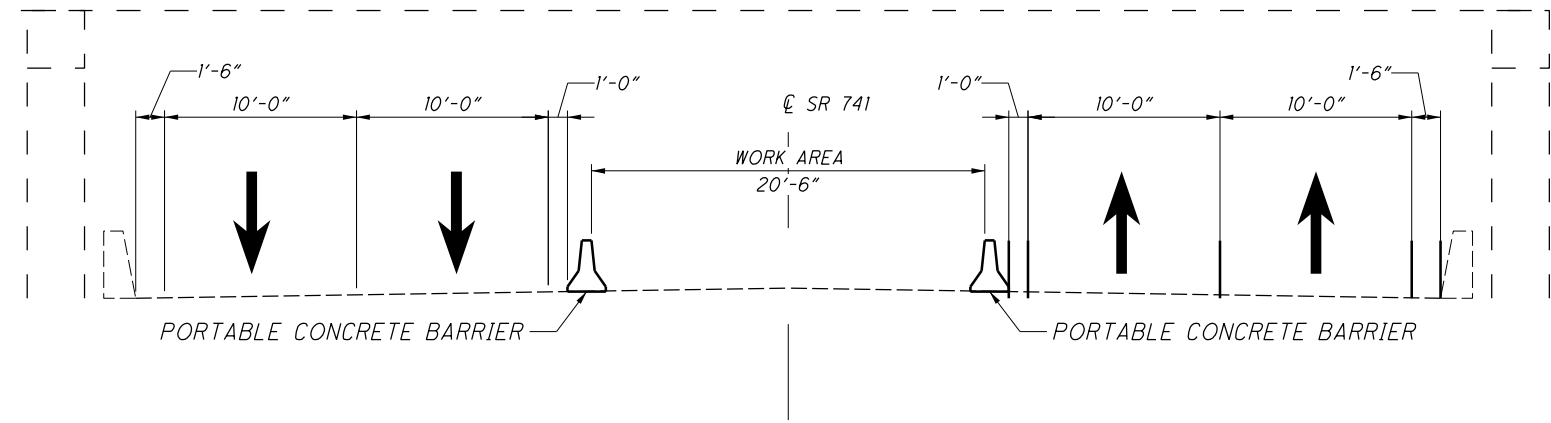
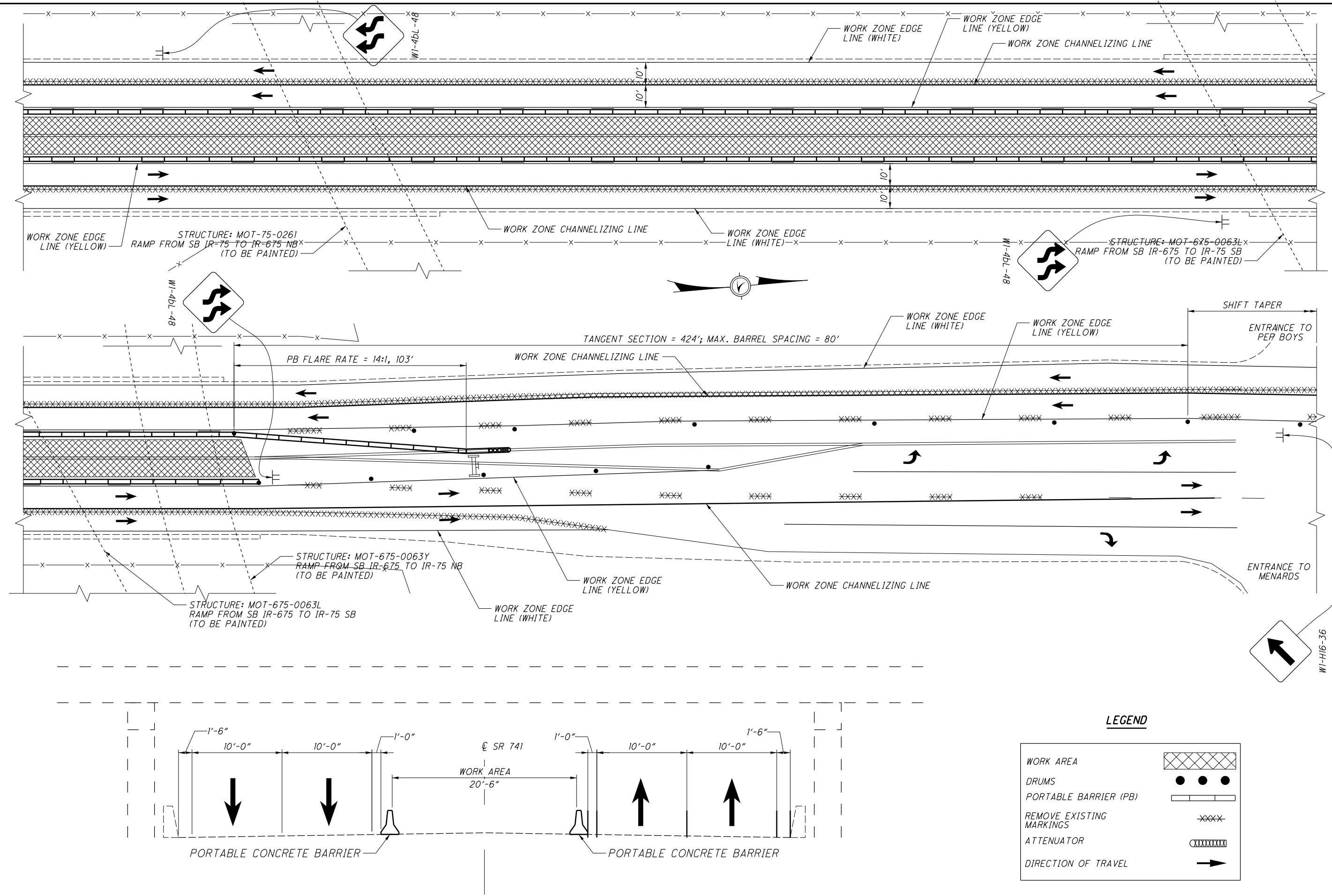
LEGEND

| | |
|--------------------------|--|
| WORK AREA | |
| DRUMS | |
| PORTABLE BARRIER (PB) | |
| REMOVE EXISTING MARKINGS | |
| ATTENUATOR | |
| DIRECTION OF TRAVEL | |

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I:\ProjectData\MOT\02303_BP_FY18\Design\Roadway\Sheets\02303_MH007.dgn Sheet 2/10/2017 8:30:51AM cweiss

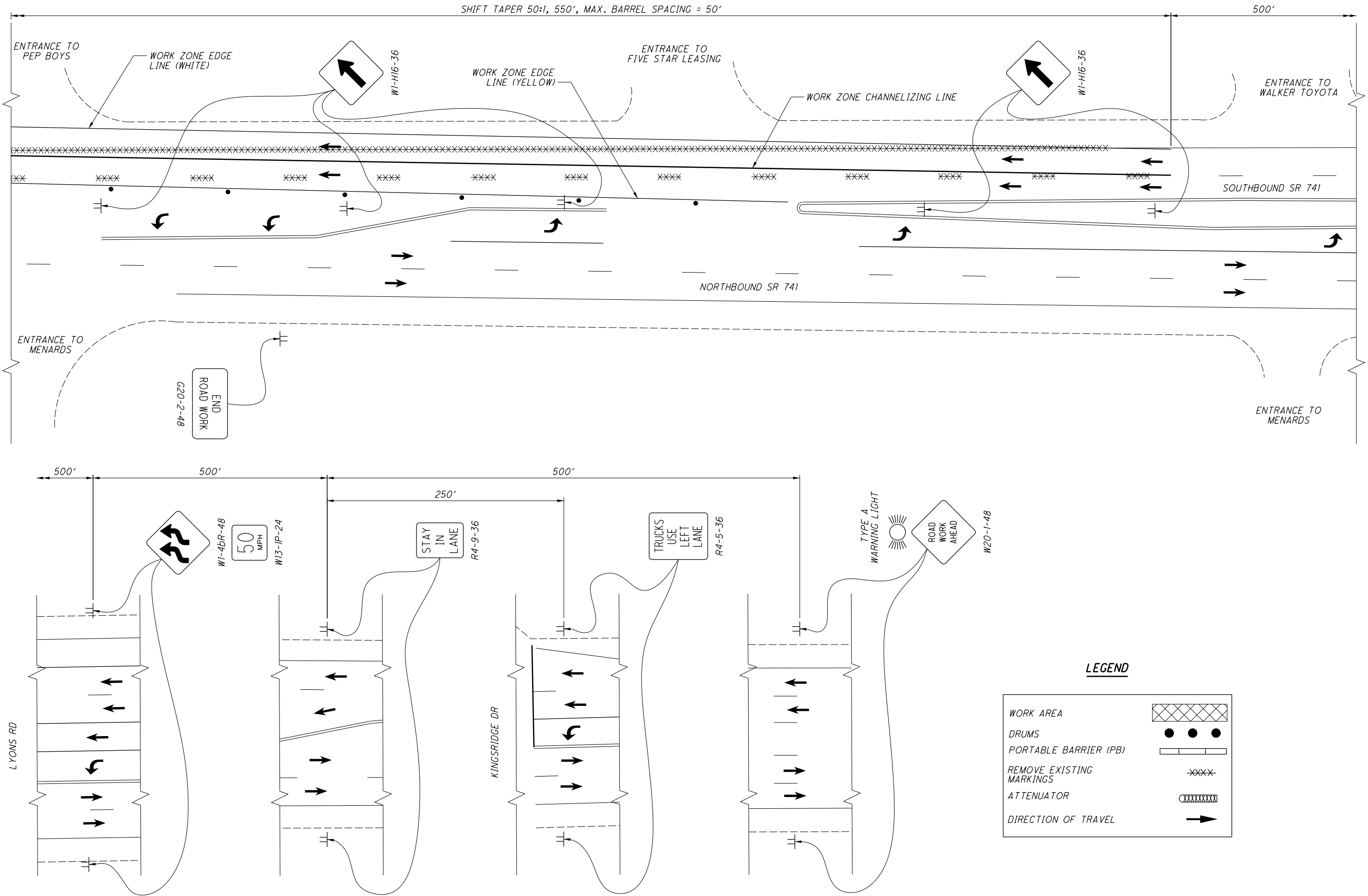


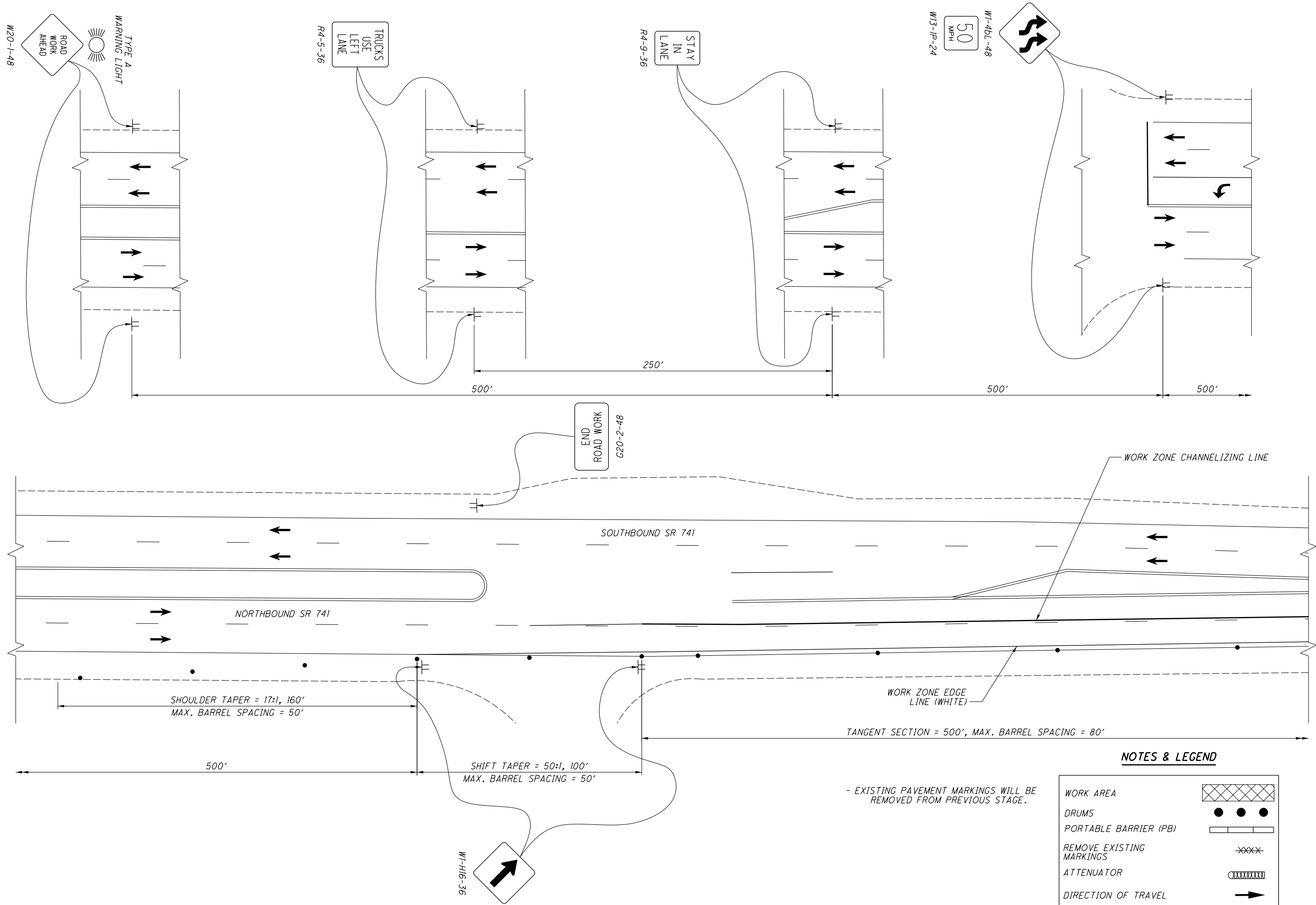
LEGEND

| | |
|--------------------------|--|
| WORK AREA | |
| DRUMS | |
| PORTABLE BARRIER (PB) | |
| REMOVE EXISTING MARKINGS | |
| ATTENUATOR | |
| DIRECTION OF TRAVEL | |

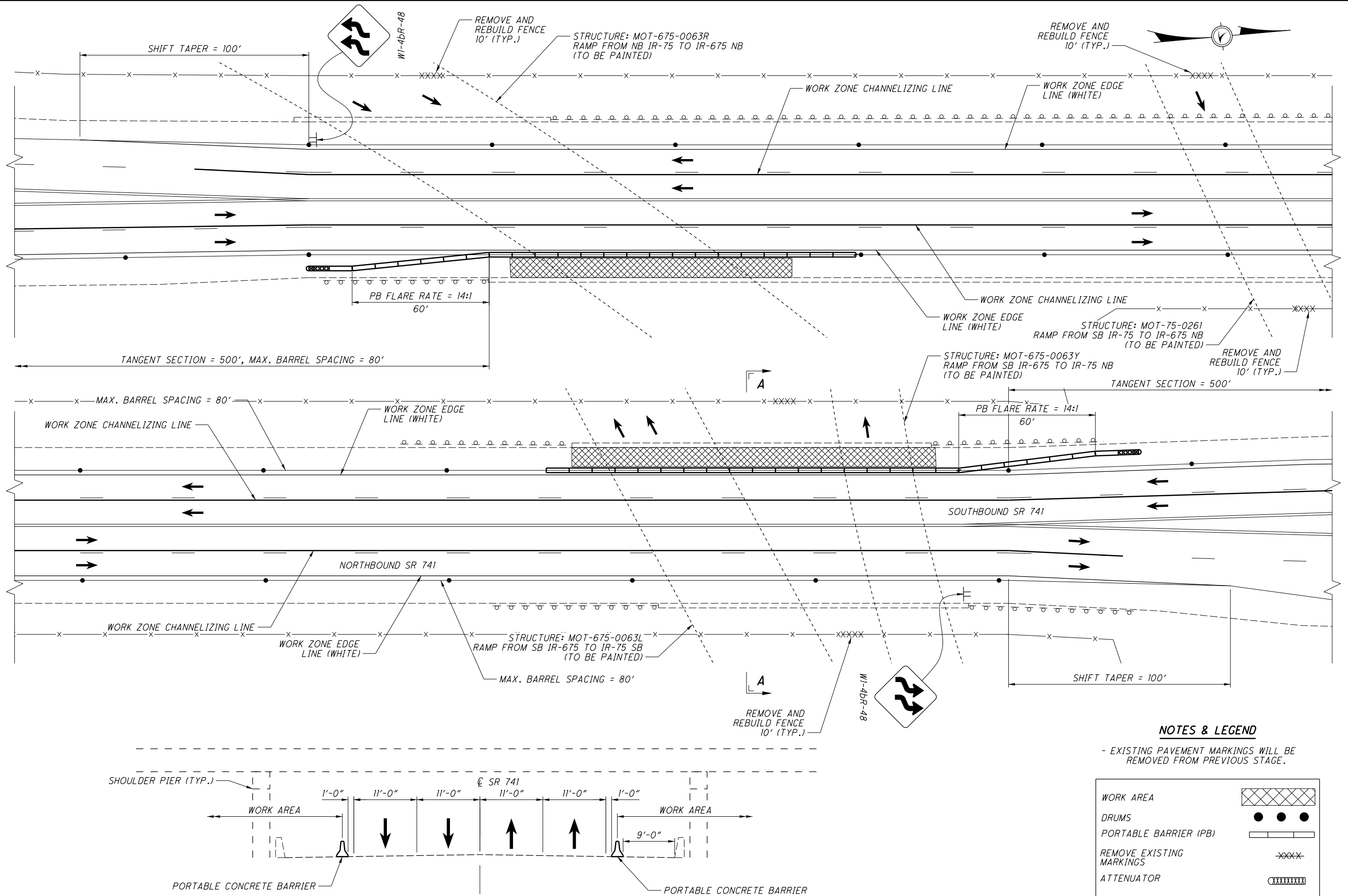
TYPICAL BRIDGE SECTION
(BRIDGE ELEMENTS NOT DRAWN TO SCALE)

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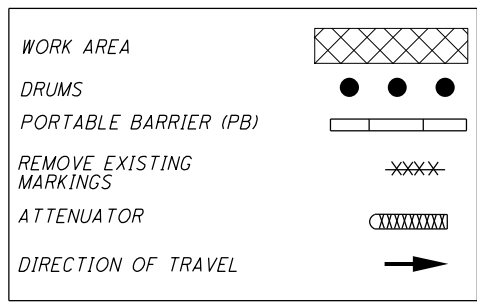
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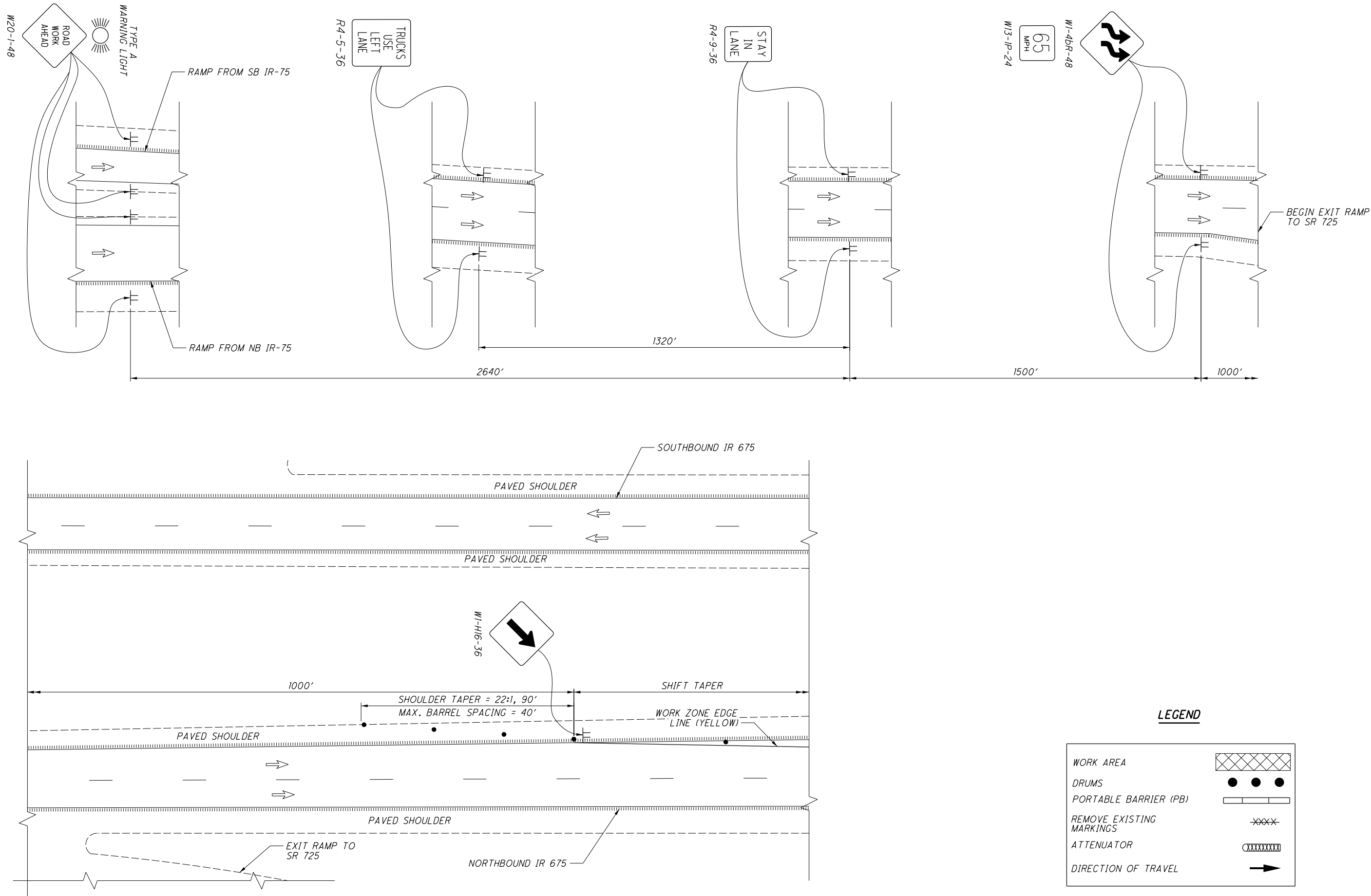


NOTES & LEGEND

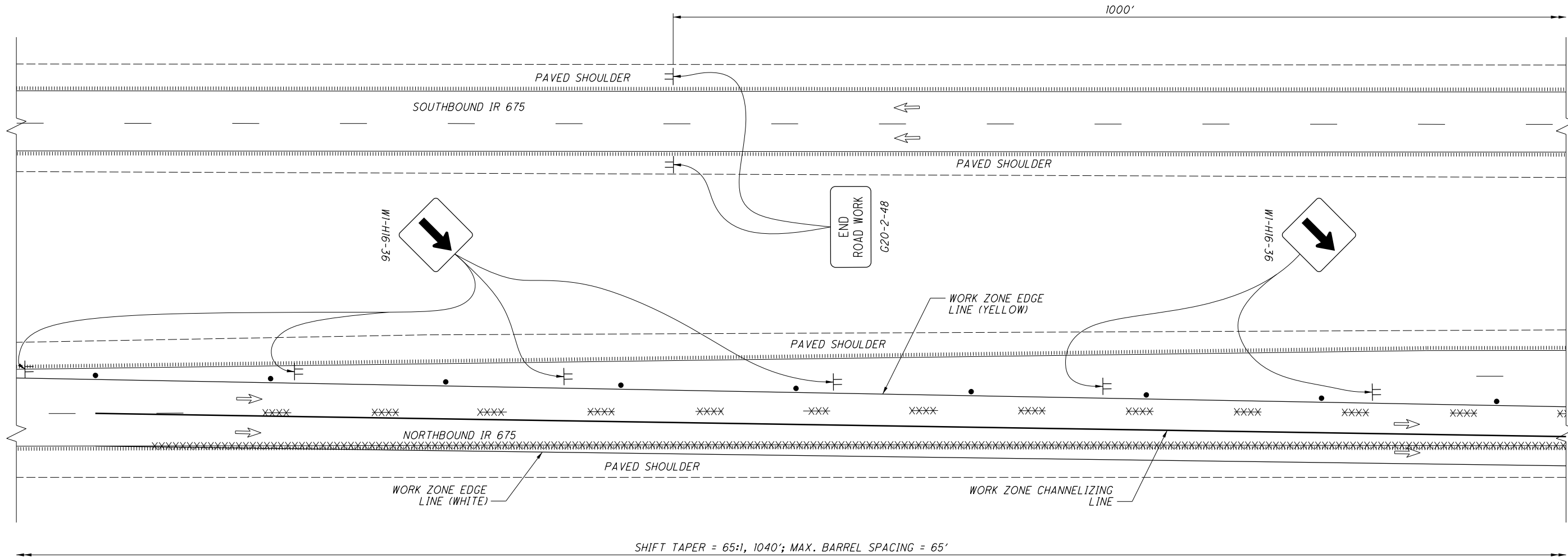
- EXISTING PAVEMENT MARKINGS WILL BE REMOVED FROM PREVIOUS STAGE.

| | |
|--------------------------|--|
| WORK AREA | |
| DRUMS | |
| PORTABLE BARRIER (PB) | |
| REMOVE EXISTING MARKINGS | |
| ATTENUATOR | |
| DIRECTION OF TRAVEL | |





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LEGEND

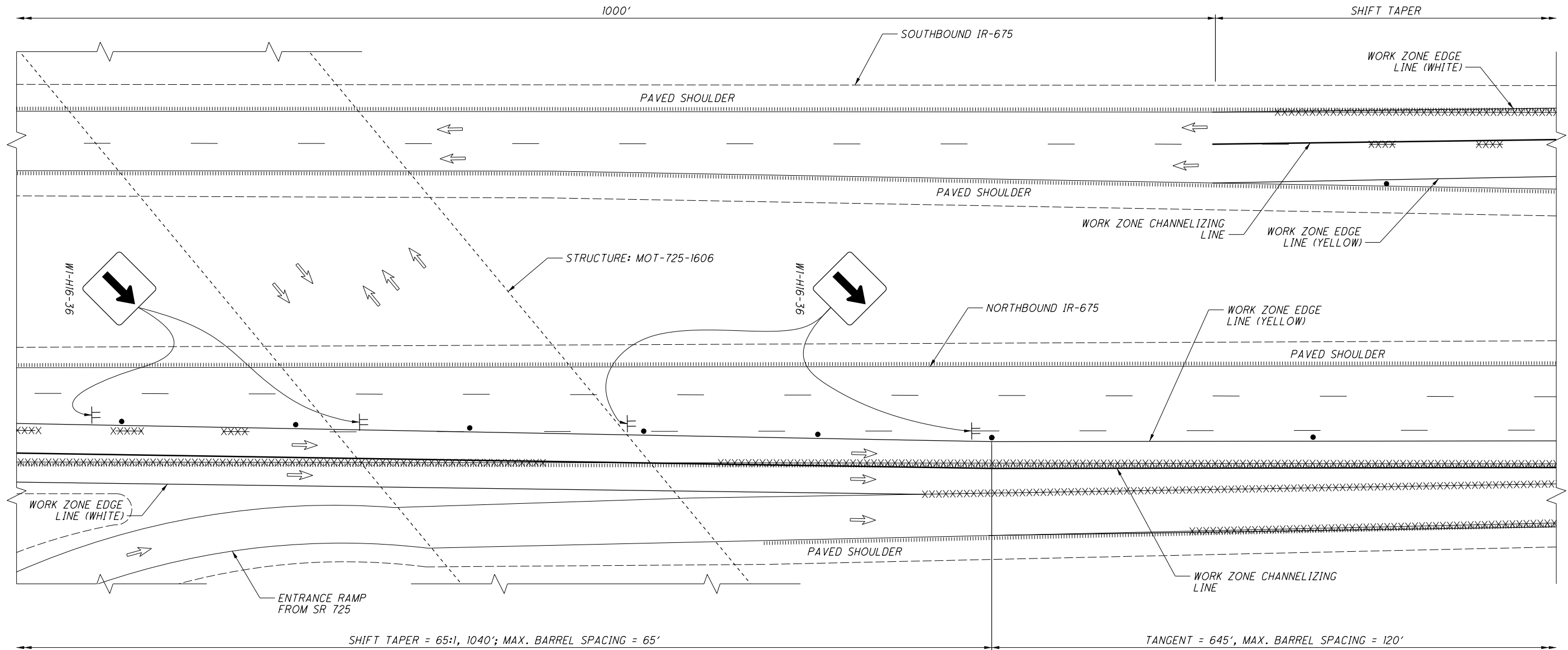
| | |
|--------------------------|--|
| WORK AREA | |
| DRUMS | |
| PORTABLE BARRIER (PB) | |
| REMOVE EXISTING MARKINGS | |
| ATTENUATOR | |
| DIRECTION OF TRAVEL | |

| | |
|------------|-----|
| CALCULATED | CWW |
| CHECKED | XXX |

MOT-675-0233 MAINTENANCE OF TRAFFIC
STAGE 1 (2 OF 9)

MOT-BP-FY18

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LEGEND

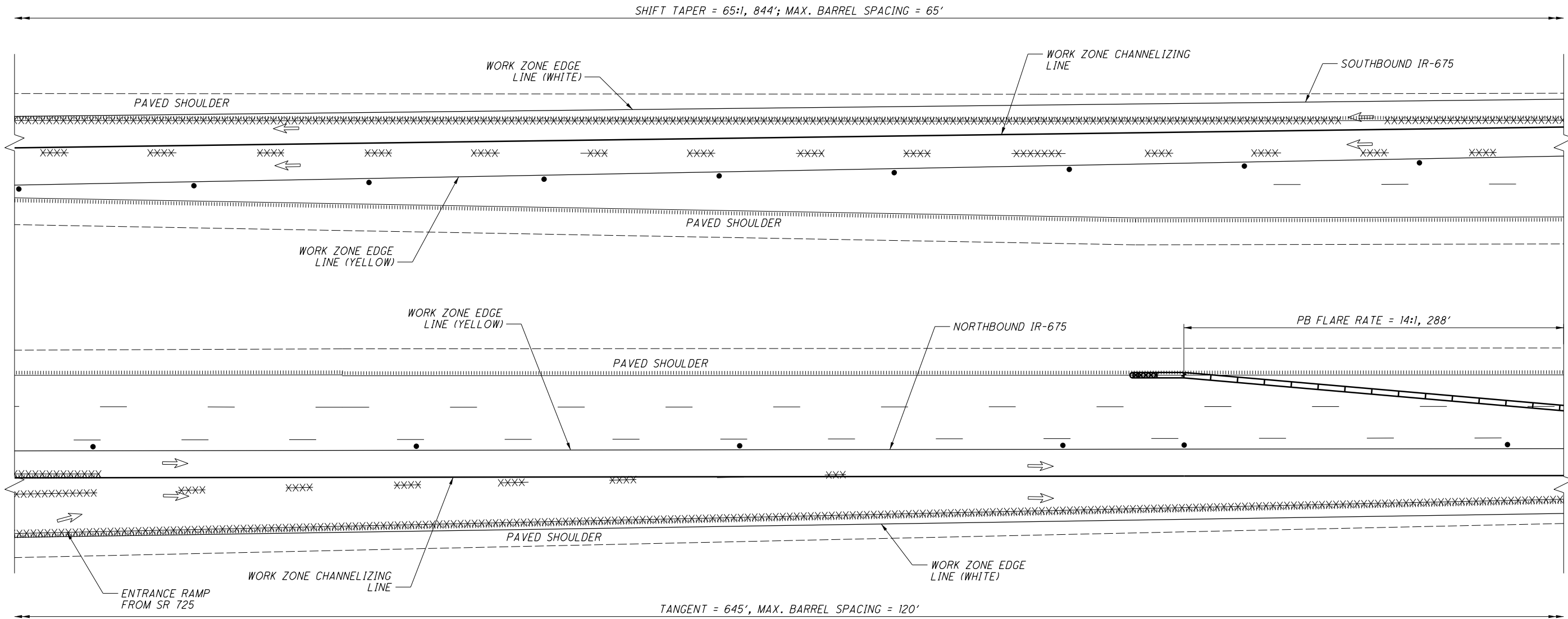
| | |
|--------------------------|--|
| WORK AREA | |
| DRUMS | |
| PORTABLE BARRIER (PB) | |
| REMOVE EXISTING MARKINGS | |
| ATTENUATOR | |
| DIRECTION OF TRAVEL | |

| | |
|------------|-----|
| CALCULATED | CWW |
| CHECKED | XXX |

MOT-675-0233 MAINTENANCE OF TRAFFIC
STAGE 1 (3 OF 9)

MOT-BP-FY18

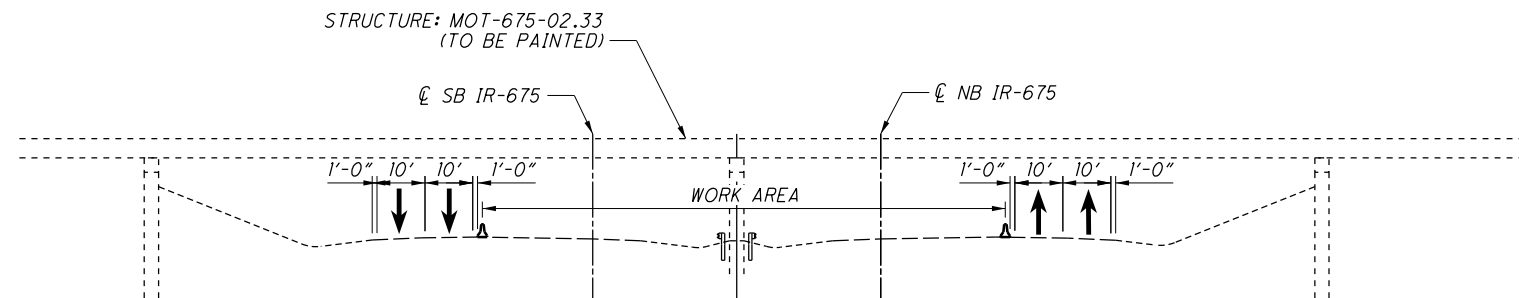
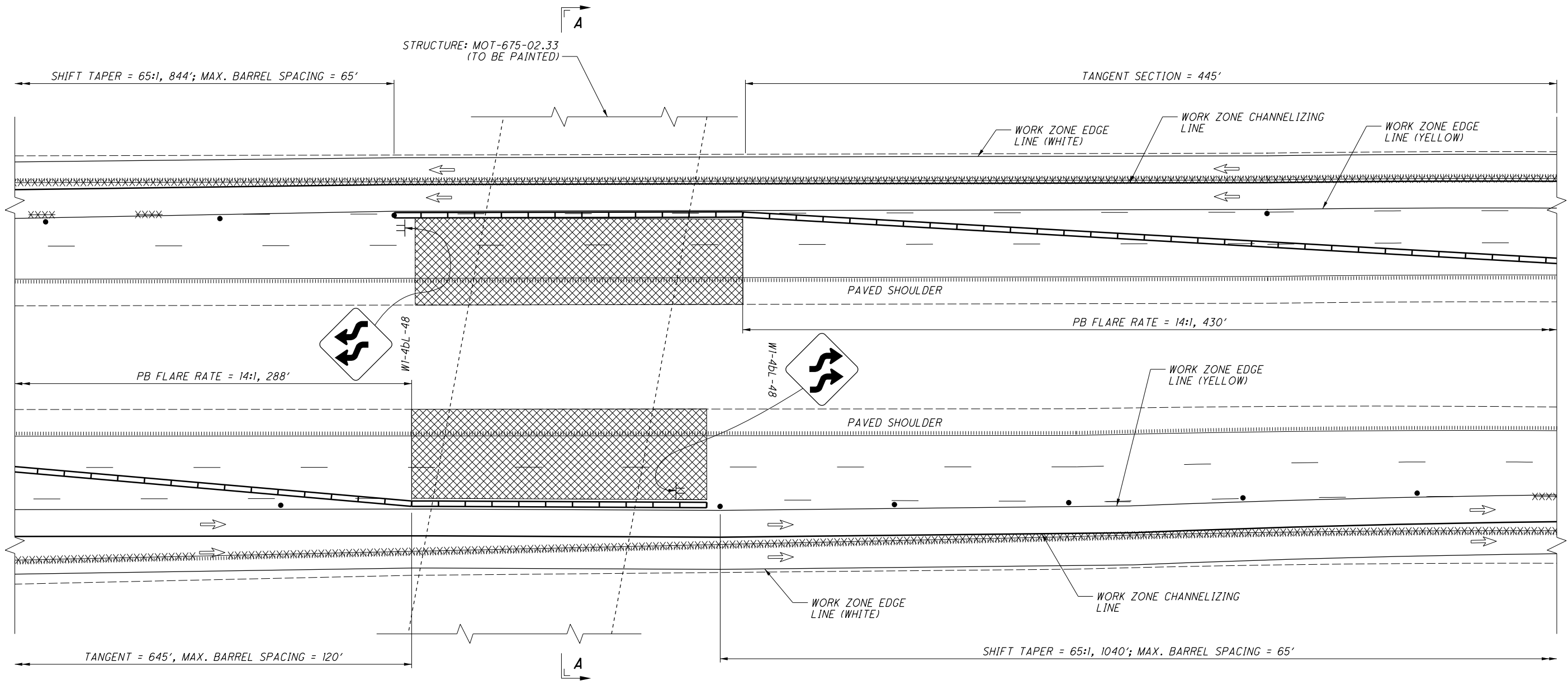
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LEGEND

| | |
|--------------------------|--|
| WORK AREA | |
| DRUMS | |
| PORTABLE BARRIER (PB) | |
| REMOVE EXISTING MARKINGS | |
| ATTENUATOR | |
| DIRECTION OF TRAVEL | |

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SECTION A-A
(BRIDGE ELEMENTS NOT DRAWN TO SCALE)

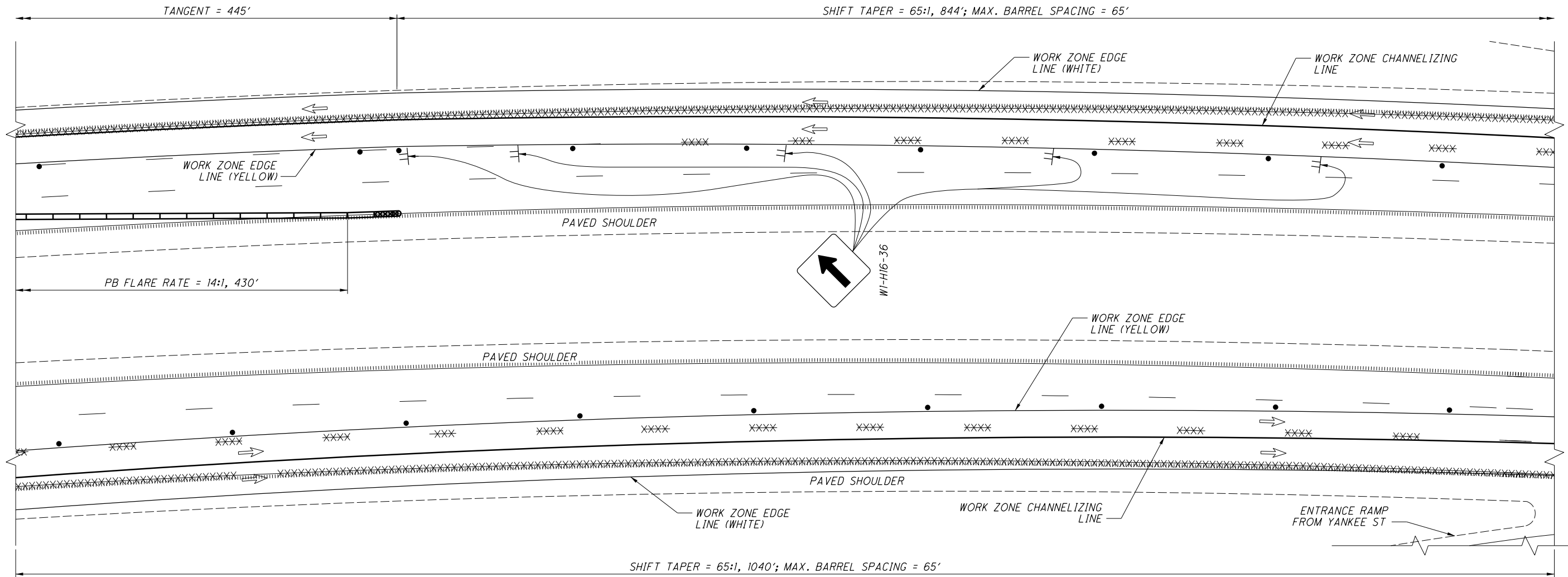
| LEGEND | |
|--------------------------|--|
| WORK AREA | |
| DRUMS | |
| PORTABLE BARRIER (PB) | |
| REMOVE EXISTING MARKINGS | |
| ATTENUATOR | |
| DIRECTION OF TRAVEL | |

| | |
|------------|-----|
| CALCULATED | CWW |
| CHECKED | XXX |

MOT-675-0233 MAINTENANCE OF TRAFFIC
STAGE 1 (5 OF 9)

MOT-BP-FY18

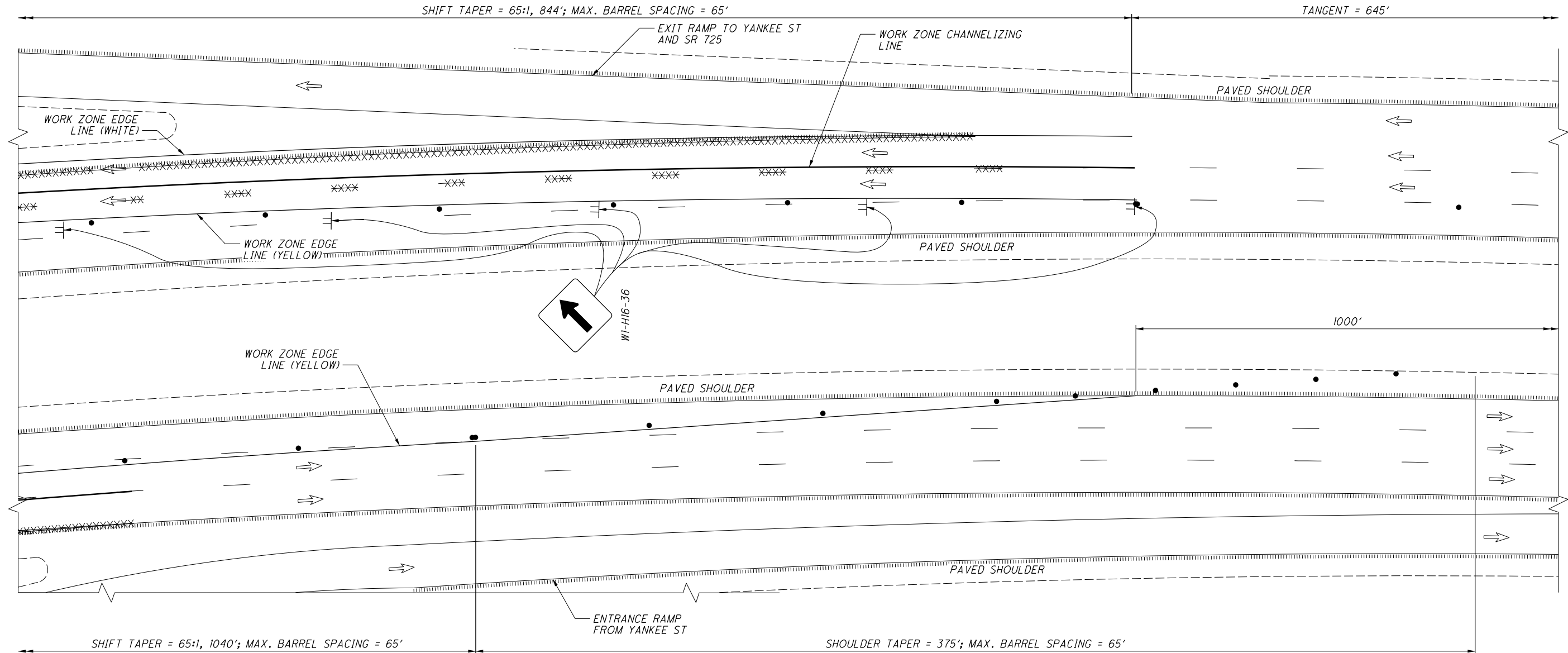
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LEGEND

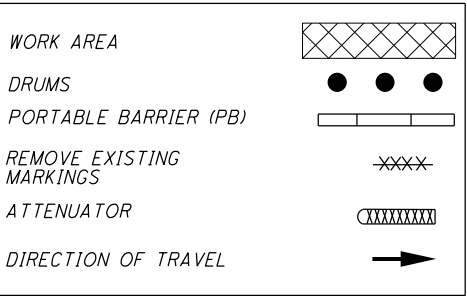
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|--------------------------|--|
| WORK AREA | |
| DRUMS | |
| PORTABLE BARRIER (PB) | |
| REMOVE EXISTING MARKINGS | |
| ATTENUATOR | |
| DIRECTION OF TRAVEL | |

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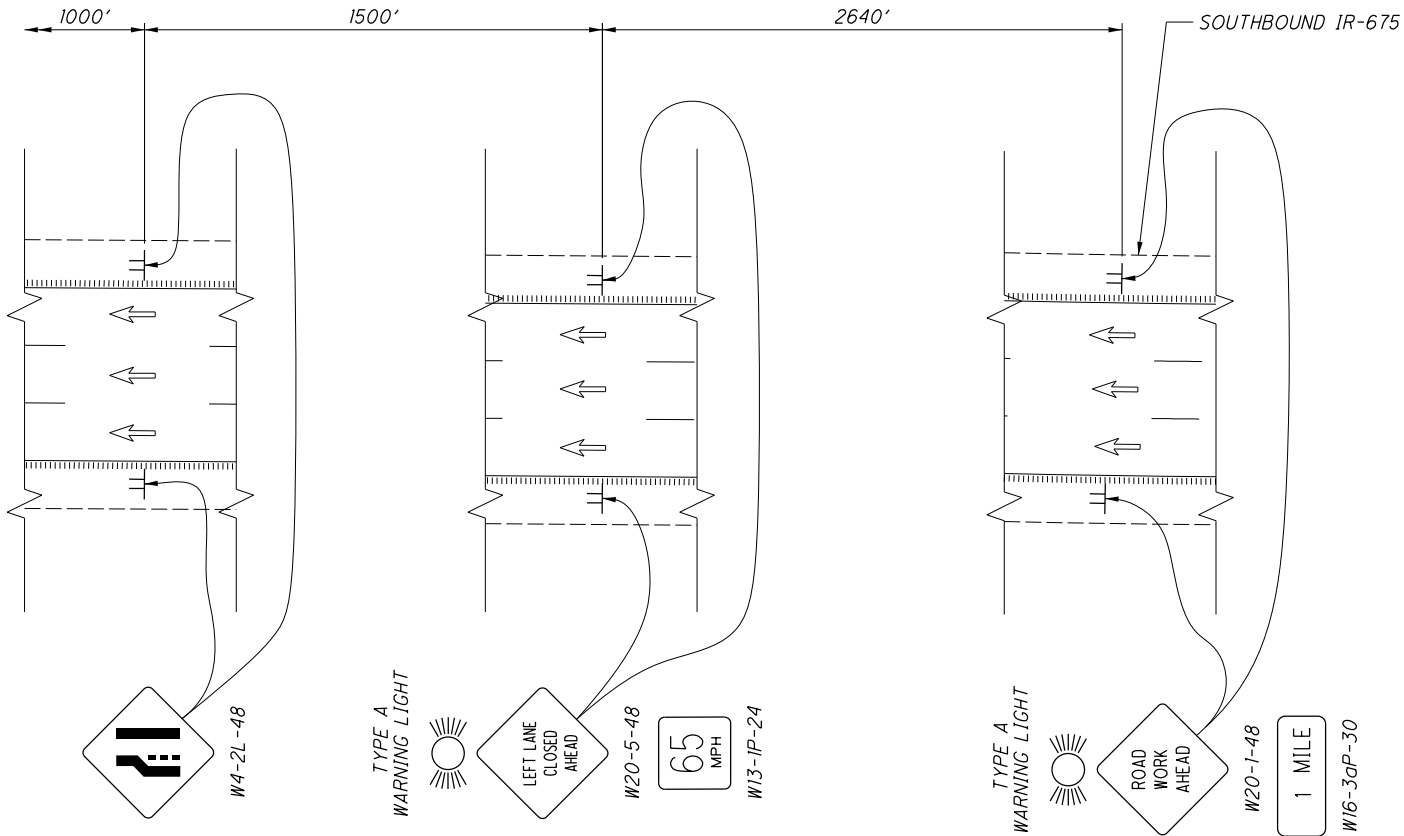


LEGEND

| | |
|--------------------------|--|
| WORK AREA | |
| DRUMS | |
| PORTABLE BARRIER (PB) | |
| REMOVE EXISTING MARKINGS | |
| ATTENUATOR | |
| DIRECTION OF TRAVEL | |



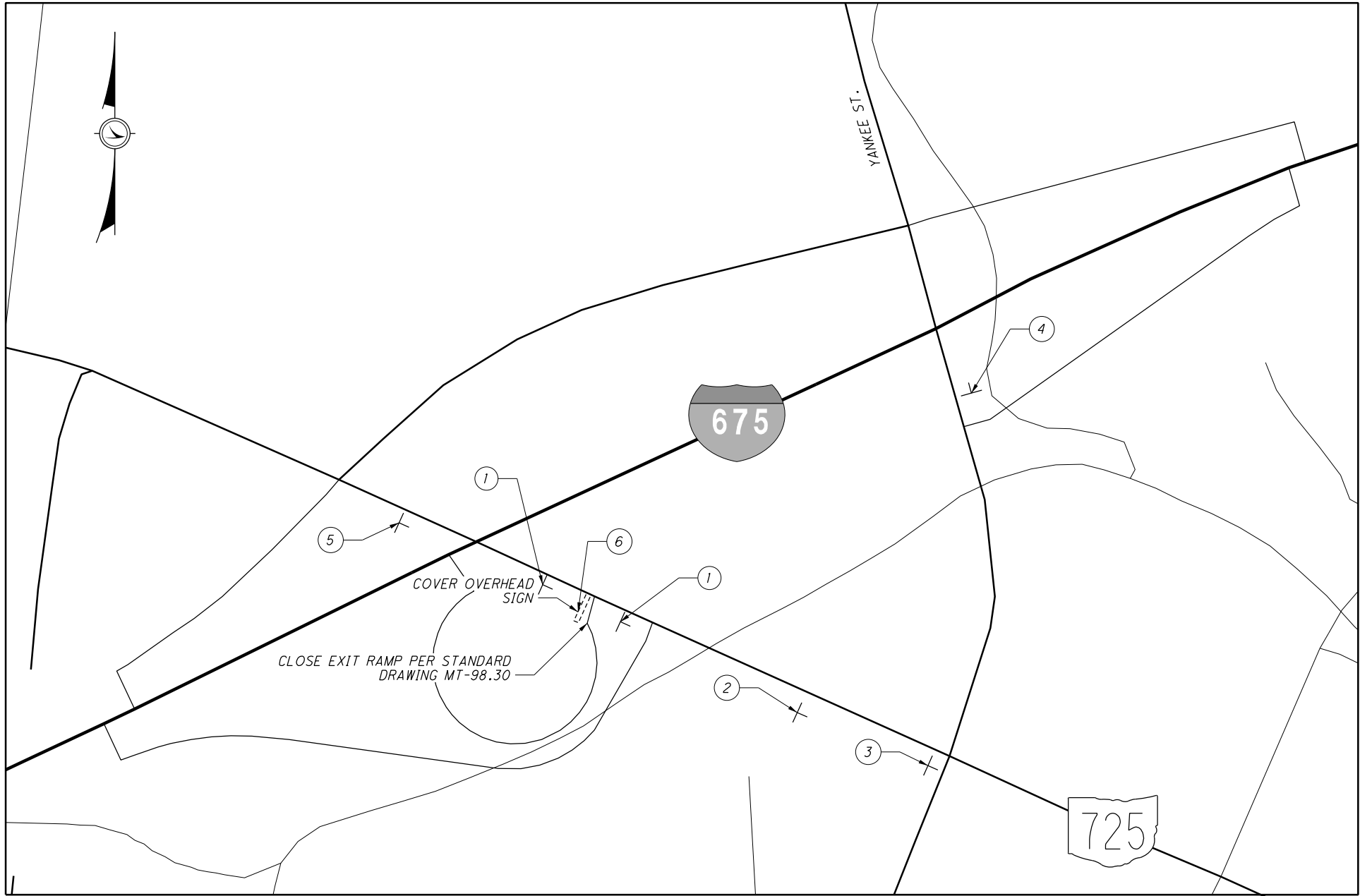
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NOTES & LEGEND

| | |
|--------------------------|--|
| WORK AREA | |
| DRUMS | |
| PORTABLE BARRIER (PB) | |
| REMOVE EXISTING MARKINGS | |
| ATTENUATOR | |
| DIRECTION OF TRAVEL | |

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DETOUR MAP

SIGN LEGEND

| | | | |
|---|---|---|---|
| 1 | DETOUR NORTH INTERSTATE 675 M4-8-24 M3-1-24 M1-1-36-3 M6-3-21 | 2 | DETOUR NORTH INTERSTATE 675 M4-8-24 M3-1-24 M1-1-36-3 M5-1-21 |
| 3 | DETOUR NORTH INTERSTATE 675 M4-8-24 M3-1-24 M1-1-36-3 M6-1-21 | 4 | END DETOUR NORTH INTERSTATE 675 M4-8a-24 M3-1-24 M1-1-36-3 |
| 5 | DETOUR AHEAD W20-2-36 | 6 | CLOSED W20-H15a-72 |

NOTES

1. THE PROJECT ENGINEER SHALL NOTIFY THE DISTRICT TRAFFIC ENGINEER PRIOR TO CLOSING THE RAMP AND WHEN THE RAMP IS OPENED.



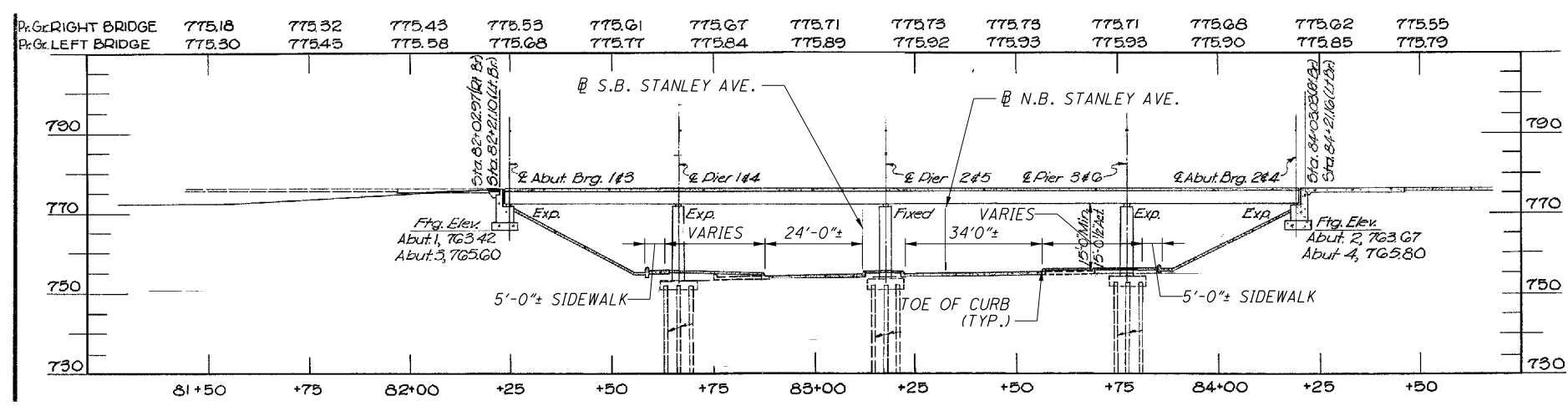
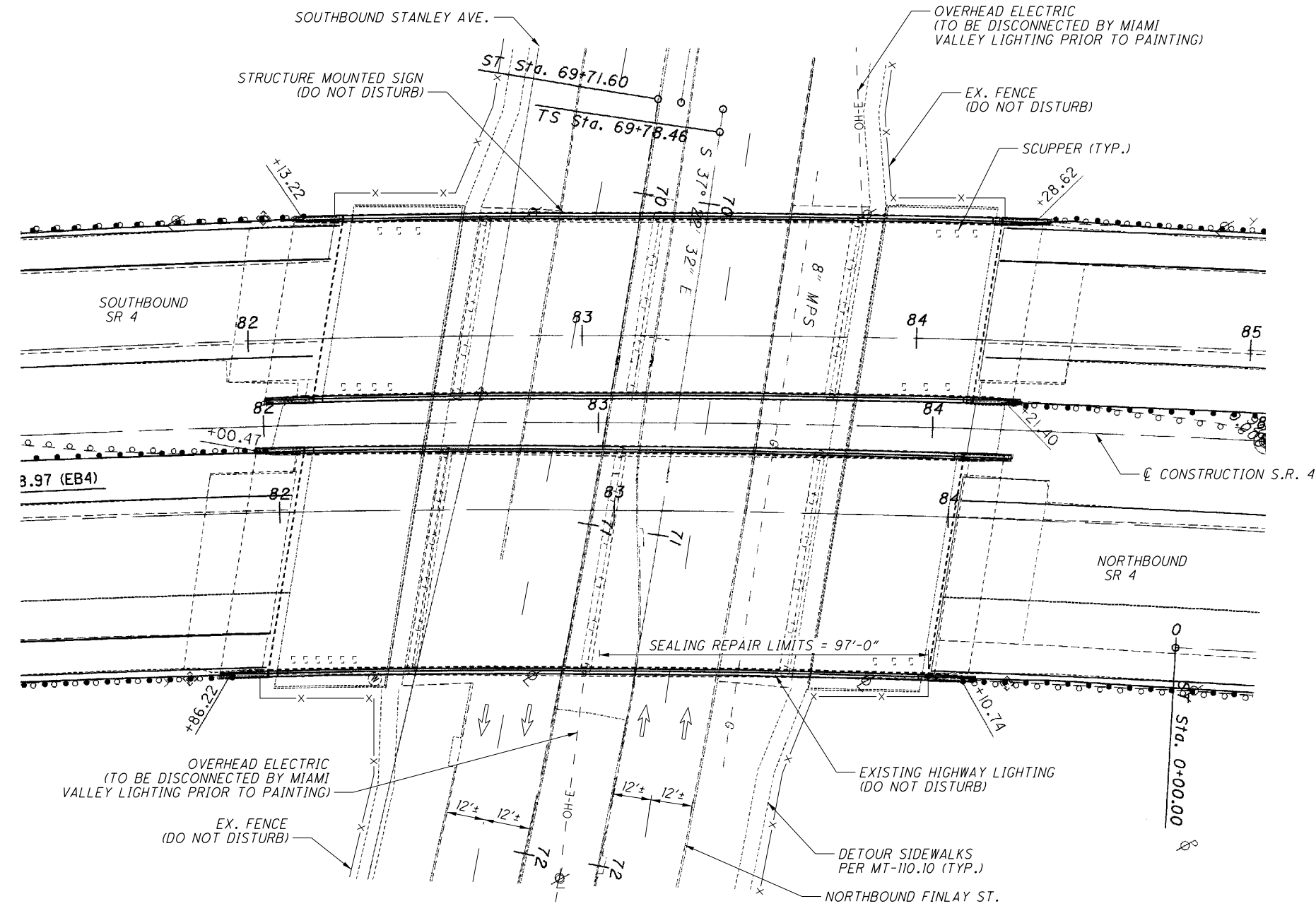
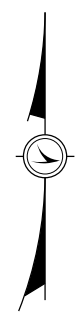
I:\Project\Data\M0T\02303_BP_FY18\Design\Roadway\Sheets\02303_GG002.dgn Sheet 11/14/2017 11:07:09 AM cweiss

| SHEET NUM. | | | | | | | | | | | | PART. | | ITEM | ITEM EXT | GRAND TOTAL | UNIT | DESCRIPTION | SEE SHEET NO. | CALCULATED CWW | CHECKED LCG |
|------------|--|--|--|--|--|--|--|--|--|--|--|-----------|-----------|---------|-------------|----------------|------|--|---------------------|-------------------|----------------|
| | | | | | | | | | | | | 01/IMS/BR | 02/NHS/BR | | | | | | | | |
| | | | | | | | | | | | | | | | | | | STRUCTURE OVER 20 FOOT SPAN (MOT-675-0055L) | | | |
| | | | | | | | | | | | | 309 | | 512 | 10100 | 309 | SY | SEALING OF CONCRETE SURFACES (EPOXY-URETHANE) | | | |
| | | | | | | | | | | | | 46,530 | | 514 | 00050 | 46,530 | SF | SURFACE PREPARATION OF EXISTING STRUCTURAL STEEL | | | |
| | | | | | | | | | | | | 46,530 | | 514 | 00056 | 46,530 | SF | FIELD PAINTING OF EXISTING STRUCTURAL STEEL, PRIME COAT | | | |
| | | | | | | | | | | | | 46,530 | | 514 | 00060 | 46,530 | SF | FIELD PAINTING STRUCTURAL STEEL, INTERMEDIATE COAT | | | |
| | | | | | | | | | | | | 46,530 | | 514 | 00066 | 46,530 | SF | FIELD PAINTING STRUCTURAL STEEL, FINISH COAT | | | |
| | | | | | | | | | | | | 23 | | 514 | 00504 | 23 | MNHR | GRINDING FINS, TEARS, SLIVERS ON EXISTING STRUCTURAL STEEL | | | |
| | | | | | | | | | | | | 25 | | 514 | 10000 | 25 | EACH | FINAL INSPECTION REPAIR | | | |
| | | | | | | | | | | | | | | | | | | STRUCTURE OVER 20 FOOT SPAN (MOT-675-0063Y) | | | |
| | | | | | | | | | | | | 9,223 | | 514 | 00050 | 9,223 | SF | SURFACE PREPARATION OF EXISTING STRUCTURAL STEEL | | | |
| | | | | | | | | | | | | 9,223 | | 514 | 00056 | 9,223 | SF | FIELD PAINTING OF EXISTING STRUCTURAL STEEL, PRIME COAT | | | |
| | | | | | | | | | | | | 9,223 | | 514 | 00060 | 9,223 | SF | FIELD PAINTING STRUCTURAL STEEL, INTERMEDIATE COAT | | | |
| | | | | | | | | | | | | 9,223 | | 514 | 00066 | 9,223 | SF | FIELD PAINTING STRUCTURAL STEEL, FINISH COAT | | | |
| | | | | | | | | | | | | 5 | | 514 | 00504 | 5 | MNHR | GRINDING FINS, TEARS, SLIVERS ON EXISTING STRUCTURAL STEEL | | | |
| | | | | | | | | | | | | 8 | | 514 | 10000 | 8 | EACH | FINAL INSPECTION REPAIR | | | |
| | | | | | | | | | | | | | | | | | | STRUCTURE OVER 20 FOOT SPAN (MOT-675-0063L) | | | |
| | | | | | | | | | | | | 15,054 | | 514 | 00050 | 15,054 | SF | SURFACE PREPARATION OF EXISTING STRUCTURAL STEEL | | | |
| | | | | | | | | | | | | 15,054 | | 514 | 00056 | 15,054 | SF | FIELD PAINTING OF EXISTING STRUCTURAL STEEL, PRIME COAT | | | |
| | | | | | | | | | | | | 15,054 | | 514 | 00060 | 15,054 | SF | FIELD PAINTING STRUCTURAL STEEL, INTERMEDIATE COAT | | | |
| | | | | | | | | | | | | 15,054 | | 514 | 00066 | 15,054 | SF | FIELD PAINTING STRUCTURAL STEEL, FINISH COAT | | | |
| | | | | | | | | | | | | 8 | | 514 | 00504 | 8 | MNHR | GRINDING FINS, TEARS, SLIVERS ON EXISTING STRUCTURAL STEEL | | | |
| | | | | | | | | | | | | 14 | | 514 | 10000 | 14 | EACH | FINAL INSPECTION REPAIR | | | |
| | | | | | | | | | | | | | | | | | | STRUCTURE OVER 20 FOOT SPAN (MOT-675-0063R) | | | |
| | | | | | | | | | | | | 38,818 | | 514 | 00050 | 38,818 | SF | SURFACE PREPARATION OF EXISTING STRUCTURAL STEEL | | | |
| | | | | | | | | | | | | 38,818 | | 514 | 00056 | 38,818 | SF | FIELD PAINTING OF EXISTING STRUCTURAL STEEL, PRIME COAT | | | |
| | | | | | | | | | | | | 38,818 | | 514 | 00060 | 38,818 | SF | FIELD PAINTING STRUCTURAL STEEL, INTERMEDIATE COAT | | | |
| | | | | | | | | | | | | 38,818 | | 514 | 00066 | 38,818 | SF | FIELD PAINTING STRUCTURAL STEEL, FINISH COAT | | | |
| | | | | | | | | | | | | 19 | | 514 | 00504 | 19 | MNHR | GRINDING FINS, TEARS, SLIVERS ON EXISTING STRUCTURAL STEEL | | | |
| | | | | | | | | | | | | 21 | | 514 | 10000 | 21 | EACH | FINAL INSPECTION REPAIR | | | |
| | | | | | | | | | | | | | | | | | | STRUCTURE OVER 20 FOOT SPAN (MOT-675-0233) | | | |
| | | | | | | | | | | | | 47,139 | | 514 | 00050 | 47,139 | SF | SURFACE PREPARATION OF EXISTING STRUCTURAL STEEL | | | |
| | | | | | | | | | | | | 47,139 | | 514 | 00056 | 47,139 | SF | FIELD PAINTING OF EXISTING STRUCTURAL STEEL, PRIME COAT | | | |
| | | | | | | | | | | | | 47,139 | | 514 | 00060 | 47,139 | SF | FIELD PAINTING STRUCTURAL STEEL, INTERMEDIATE COAT | | | |
| | | | | | | | | | | | | 47,139 | | 514 | 00066 | 47,139 | SF | FIELD PAINTING STRUCTURAL STEEL, FINISH COAT | | | |
| | | | | | | | | | | | | 24 | | 514 | 00504 | 24 | MNHR | GRINDING FINS, TEARS, SLIVERS ON EXISTING STRUCTURAL STEEL | | | |
| | | | | | | | | | | | | 26 | | 514 | 10000 | 26 | EACH | FINAL INSPECTION REPAIR | | | |
| | | | | | | | | | | | | | | | | | | MAINTENANCE OF TRAFFIC | | | |
| | | | | | | | | | | | | 848 | 32 | 614 | 11110 | 880 | hour | LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE | | | |
| | | | | | | | | | | | | 2,300 | | 614 | 11630 | 2,300 | FT | INCREASED BARRIER DELINEATION | | | |
| | | | | | | | | | | | | 13 | | 614 | 12336 | 13 | EACH | WORK ZONE IMPACT ATTENUATOR (UNIDIRECTIONAL) | | | |
| | | | | | | | | | | | | | LS | 614 | 12420 | LS | | DETOUR SIGNING | | | |
| | | | | | | | | | | | | 4 | | 614 | 12500 | 4 | EACH | REPLACEMENT SIGN | | | |
| | | | | | | | | | | | | 8 | 2 | 614 | 12600 | 10 | EACH | REPLACEMENT DRUM | | | |
| | | | | | | | | | | | | 2 | 2 | SPECIAL | 61412760 | 4 | EACH | FLASHING ARROW PANEL | | | |
| | | | | | | | | | | | | 175 | | 614 | 13310 | 175 | EACH | BARRIER REFLECTOR, TYPE 1, ONE WAY | | | |
| | | | | | | | | | | | | 21 | | 614 | 13312 | 21 | EACH | BARRIER REFLECTOR, TYPE 2, ONE WAY | | | |
| | | | | | | | | | | | | 196 | | 614 | 13350 | 196 | EACH | OBJECT MARKER, ONE WAY | | | |
| | | | | | | | | | | | | 11 | 1 | 614 | 18600 | 12 | SNMT | PORTABLE CHANGEABLE MESSAGE SIGN | | | |
| | | | | | | | | | | | | 8.49 | 0.38 | 614 | 22360 | 8.87 | MILE | WORK ZONE EDGE LINE, CLASS III, 6", 642 PAINT | | | |
| | | | | | | | | | | | | 33,947 | | 614 | 23690 | 33,947 | FT | WORK ZONE CHANNELIZING LINE, CLASS III, 12", 642 PAINT | | | |
| | | | | | | | | | | | | 7,270 | | 622 | 41000 | 7,270 | FT | PORTABLE BARRIER, 32" | | | |
| | | | | | | | | | | | | | | | | | | INCIDENTALS | | | |
| | | | | | | | | | | | | LS | LS | 614 | 11000 | LS | | MAINTAINING TRAFFIC | | | |
| | | | | | | | | | | | | LS | LS | 624 | 10000 | LS | | MOBILIZATION | | | |

GENERAL SUMMARY

MOT-BP-FY18

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NOTES

DETAILS SHOWN ARE TAKEN FROM EXISTING PLANS AND ARE FOR REFERENCE ONLY.

ALL DIMENSIONS CONSIDERED APPROXIMATE.

EXISTING STRUCTURE (MOT-4-1842 L)

TYPE: CONTINUOUS STEEL BEAMS WITH REINFORCED CONCRETE DECK AND SUBSTRUCTURE.

SPANS: 42'-0", 52'-0", 59'-6", 42'-0"
ROADWAY: 52'-8" TOE/TOE PARAPETS
LOADING: CF 2000(CF)
SKEW: 9°-5'-53" LT. FWD.
APPROACH SLABS: AS-1-54 (25' LONG)
ALIGNMENT: 1°-30' CURVE TO THE RIGHT
CROWN: SUPERELEVATION 0.036'/FT
STRUCTURAL FILE NUMBER: 5700787
DATE BUILT: 1960
DISPOSITION: PAINTING STRUCTURAL STEEL

EXISTING STRUCTURE (MOT-4-1842 R)

TYPE: CONTINUOUS STEEL BEAMS WITH REINFORCED CONCRETE DECK AND SUBSTRUCTURE.

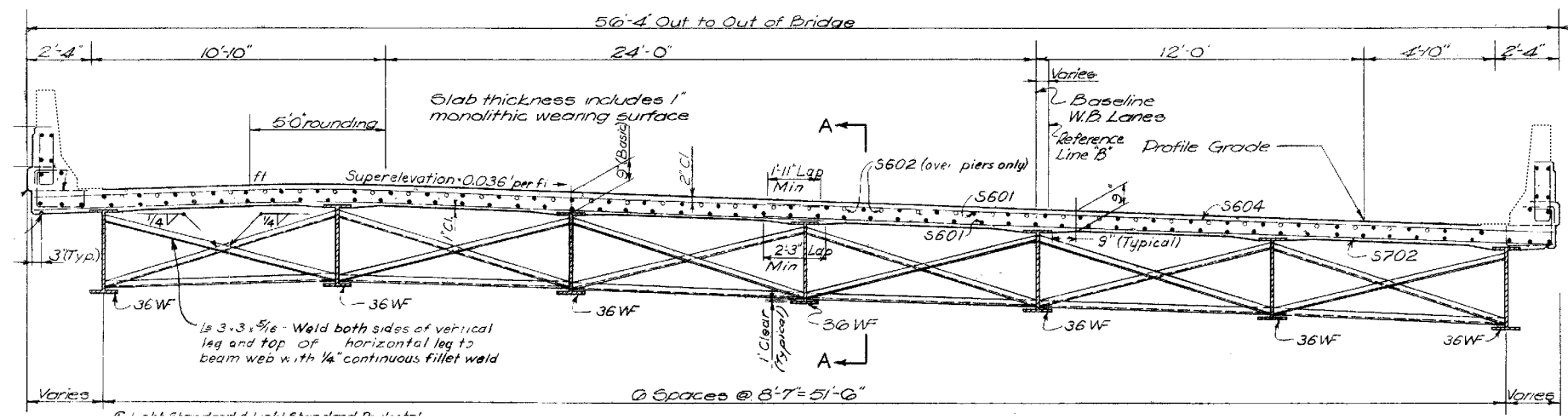
SPANS: 42'-0", 52'-0", 59'-6", 42'-0"
ROADWAY: 64'-8" TOE/TOE PARAPETS
LOADING: CF 2000(CF)
SKEW: 8°-43'-13" LT. FWD.
APPROACH SLABS: AS-1-54 (25' LONG)
ALIGNMENT: 1°-30' CURVE TO THE RIGHT
CROWN: SUPERELEVATION 0.036'/FT
STRUCTURAL FILE NUMBER: 5700817
DATE BUILT: 1960
DISPOSITION: PAINTING STRUCTURAL STEEL

PROPOSED WORK (MOT-4-18.42 L/R)

TYPE: PAINT STRUCTURAL STEEL, BEAMS, CROSS FRAMES, END FRAMES, AND BEARINGS. SEAL THE DECK PARAPETS AS SHOWN WITH EPOXY URETHANE.

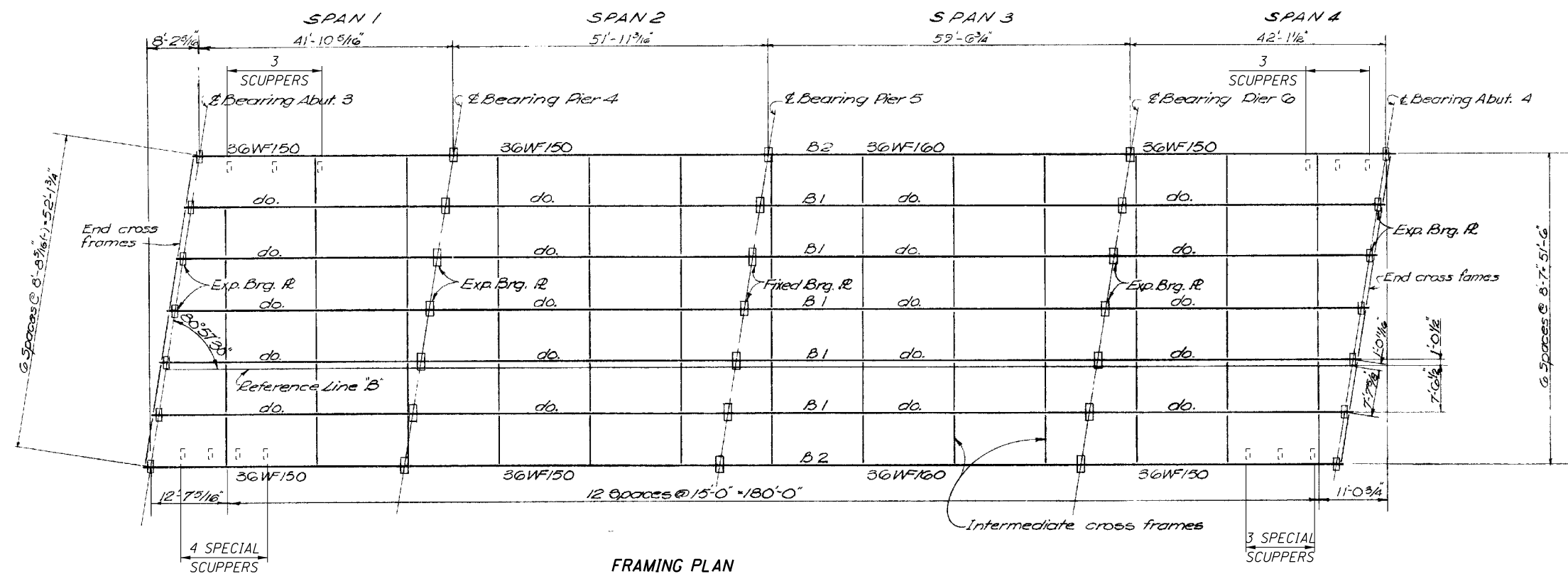
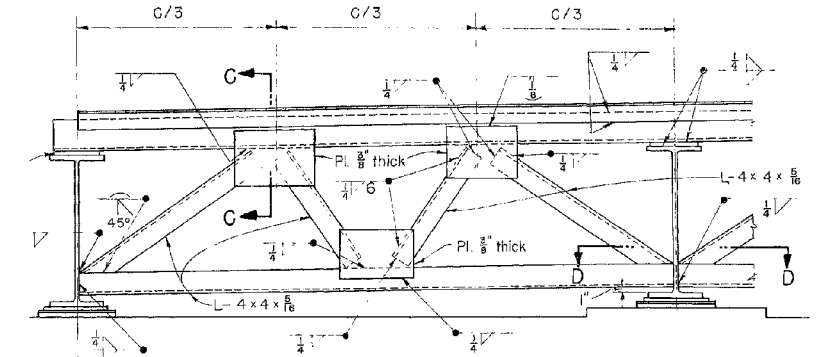
WORK LISTED IS NOT ALL INCLUSIVE. CONTRACTOR WILL SEQUENCE WORK AS NEEDED.

| | | | | | | | |
|------------------------|---------|----------|-------|----------|--------------------------|-----------|--|
| DESIGN AGENCY | DATE | REVIEWED | DRAWN | DESIGNED | MONTGOMERY COUNTY | SITE PLAN | ODOT DISTRICT 7 |
| PLANNING & ENGINEERING | 1/31/17 | MRB | MEF | MEF | STA. 81+00 STA. 85+00 | | BRIDGE NO.: MOT-4-1842 L/R SR 4 OVER STANLEY AVENUE |
| | | | | | | | MOT-BP-FY18 |
| | | | | | | | PID No. 102303 |
| | | | | | | | 1 / 3 |
| | | | | | | | 49 71 |

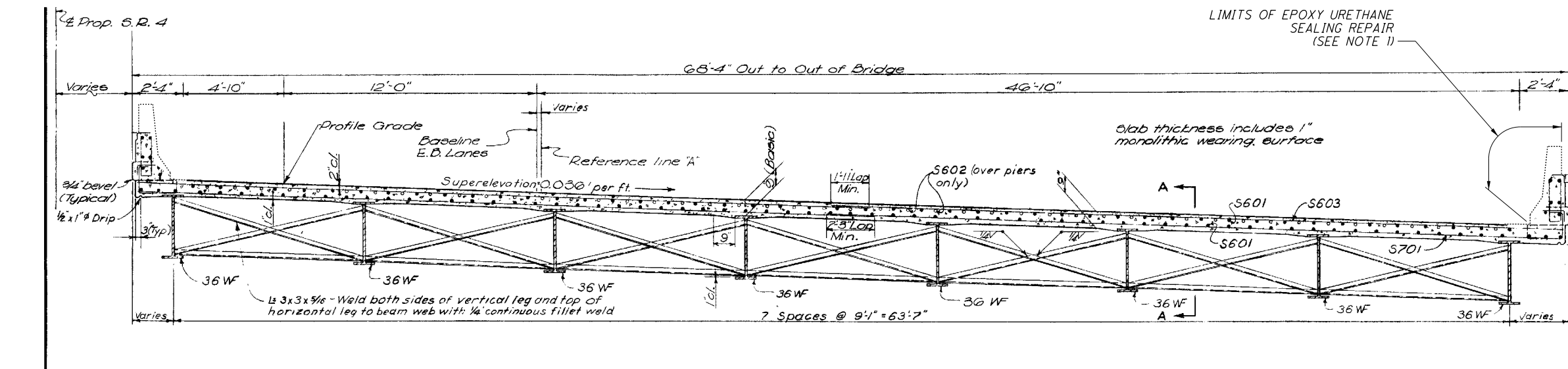


NOTES

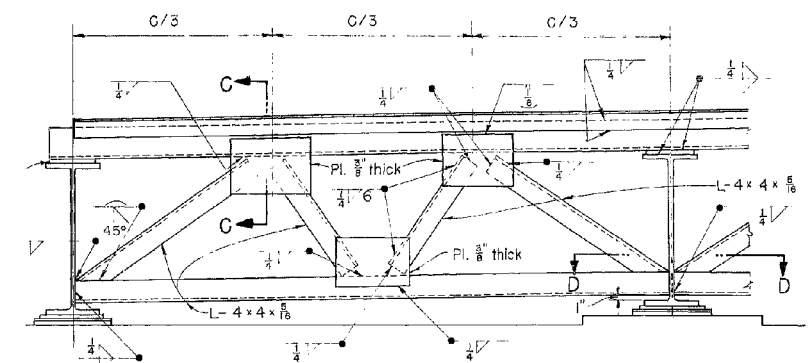
1. ALL SCUPPERS ARE 6" ϕ PIPE. THE SPECIAL SCUPPERS HAVE AN OFFSET DOWN PIPE.



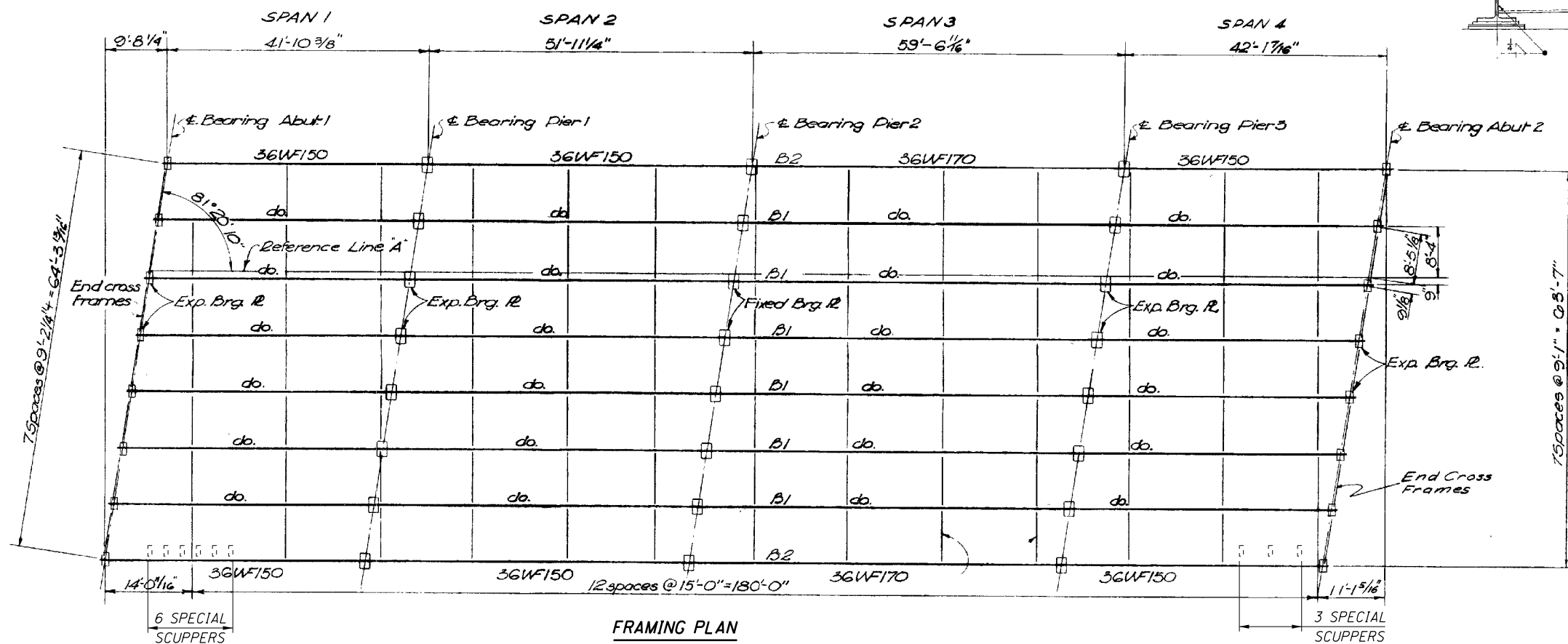
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MOT-4-1842 R TRANSVERSE SECTION



ENDFRAME DETAIL



FRAMING PLAN

NOTES

1. THE EXISTING FAILED EPOXY URETHANE SEALER ON THE RIGHT PARAPET FACE BETWEEN THE EXISTING LIGHT POLE AND THE FORWARD ABUTMENT SHALL BE REMOVED AND RESEALED PER CMS 512.
2. ALL SCUPPERS ARE 6" Ø PIPE. THE SPECIAL SCUPPERS HAVE AN OFFSET DOWN PIPE.

SUPERSTRUCTURE DETAILS

BRIDGE NO.: MOT-4-1842 R
SR 4 OVER STANLEY AVENUE

MOT-BP-FY18

PID No. 102303

3 / 3

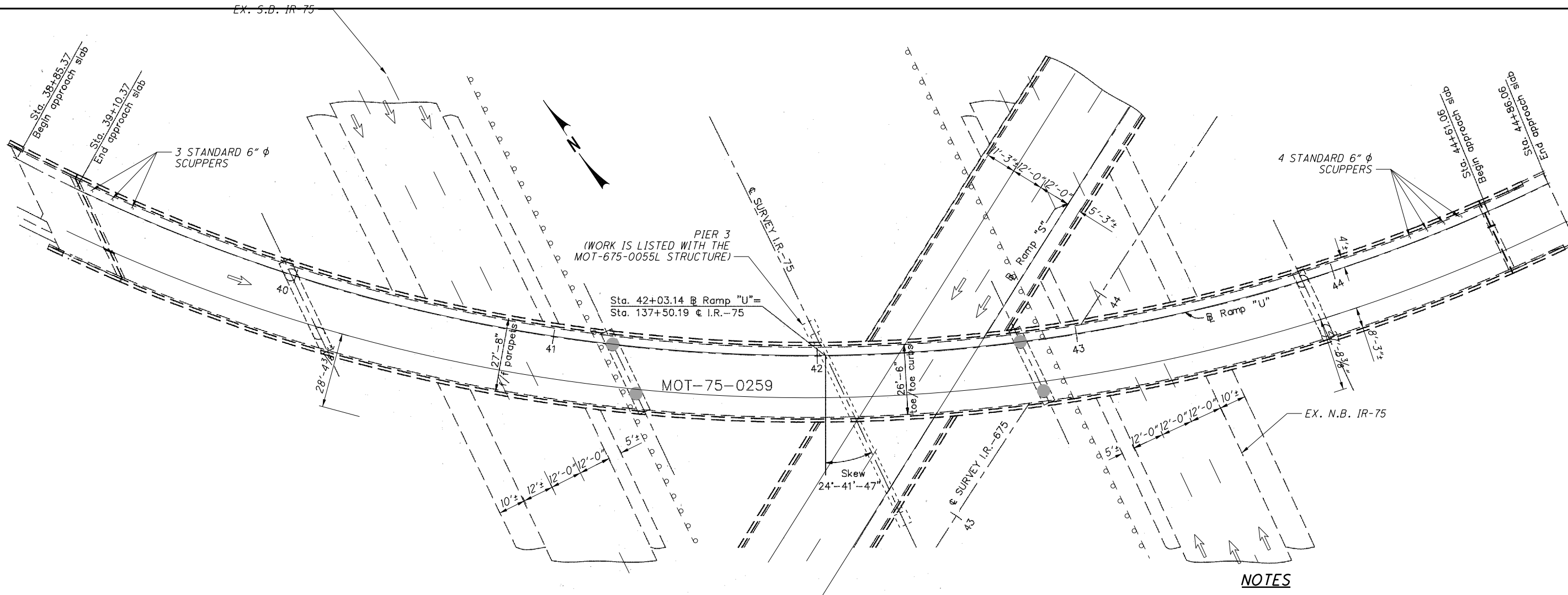
51
71

DESIGN AGENCY
ODOT DISTRICT 7
PLANNING & ENGINEERING

REVIEWED
MRB
DATE
1/31/17
STRUCTURE FILE NUMBER
5700787/5700817

DRAWN
CWW
CHECKED
DESIGNED
CWW

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PLAN

NOTES

DETAILS SHOWN ARE TAKEN FROM EXISTING PLANS AND ARE FOR REFERENCE ONLY.

ALL DIMENSIONS CONSIDERED APPROXIMATE.

1. PIER COLUMNS ARE 4'-6"± IN DIAMETER. THE AVERAGE EXPOSED COLUMN HEIGHT FOR PIER 2 IS 39'-10"± AND PIER 4 IS 39'-2"±.

● - PIER COLUMNS TO BE SEALED PER ITEM 512 SEALING OF CONCRETE SURFACES (EPOXY-URETHANE)

PROPOSED WORK

TYPE: PAINT STRUCTURAL STEEL, BEAMS, CROSS FRAMES, END FRAMES, SCUPPERS AND BEARINGS. SEAL THE PIER COLUMNS AS SHOWN WITH EPOXY URETHANE.

EXISTING STRUCTURE

TYPE: CONTINUOUS PLATE GIRDER WITH REINFORCED CONCRETE DECK AND SUBSTRUCTURE.

SPANS: 84'.0', 124.63', 80.52', 75.53', 109.0', 71.0'

ROADWAY: 26'-6" TOE/TOE PARAPET

LOADING: HS20-44 CASE II & THE ALT. MILITARY LOADING

SKEW: 24°-41'-47" RT. FWD.

APPROACH SLABS: AS-1-72 (25' LONG)

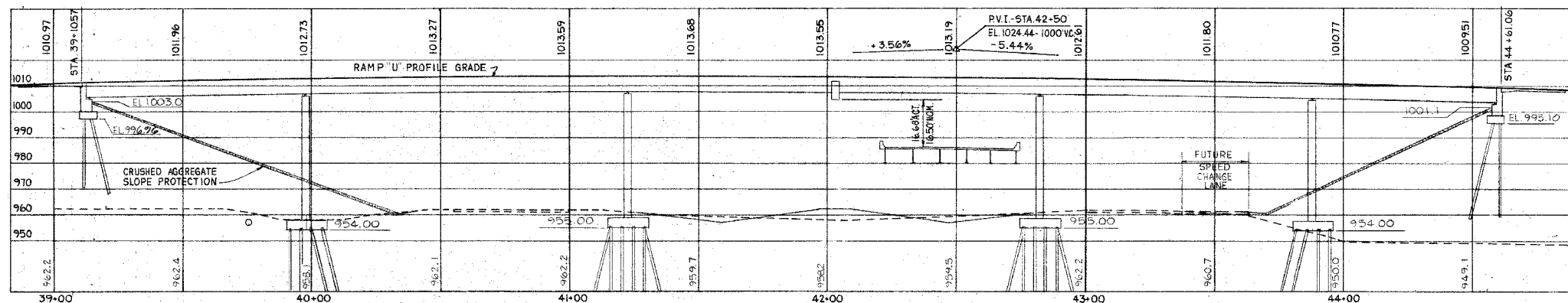
ALIGNMENT: 9° CURVE TO THE LEFT

CROWN: SUPERELEVATION (VAR.)

STRUCTURAL FILE NUMBER: 5706432

DATE BUILT: 1958

DISPOSITION: PAINTING STRUCTURAL STEEL



ELEVATION VIEW

DESIGN AGENCY
ODOT DISTRICT 7
PLANNING & ENGINEERING

DATE
1/31/17
REVIEWED
MRB
STRUCTURE FILE NUMBER
5706432

DRAWN
MEF
DESIGNED
MEF
CHECKED
CWW

MONTGOMERY COUNTY
STA. 38+85
STA. 44+86

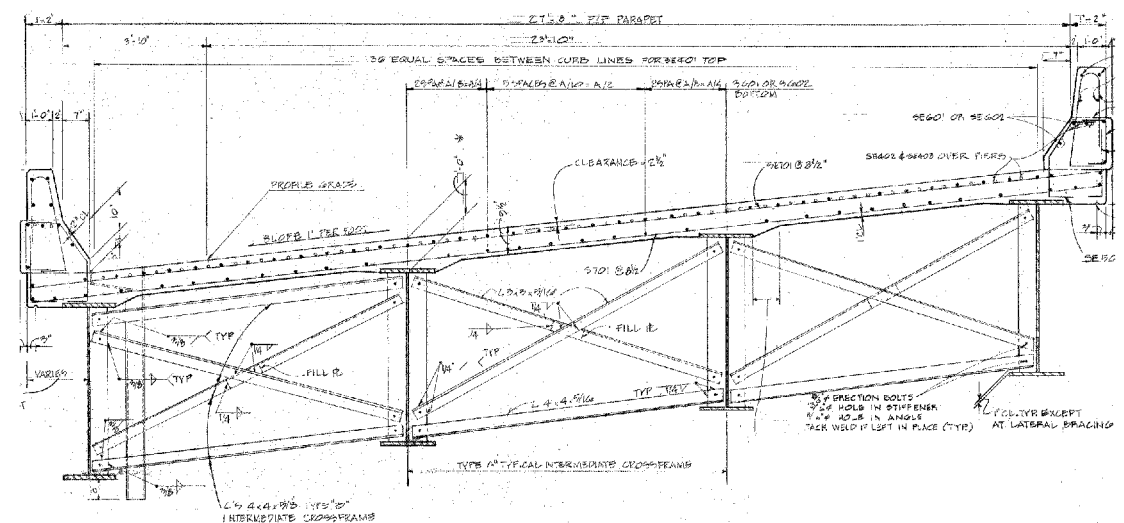
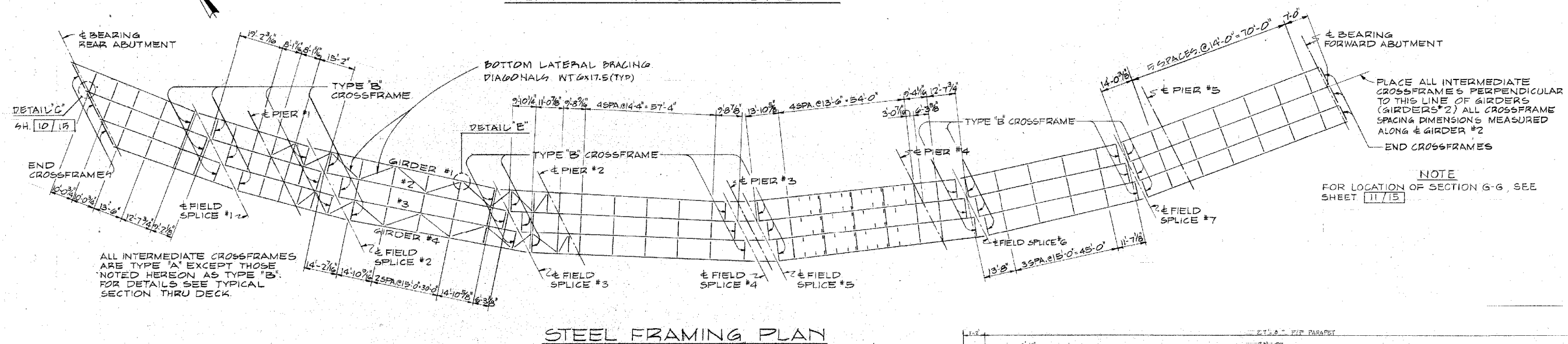
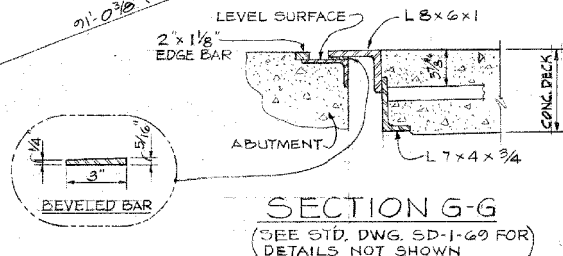
SITE PLAN

BRIDGE NO.: MOT-75-0259
IR-675 RAMP "U" OVER IR-75

MOT-BP-FY18
PID No. 102303

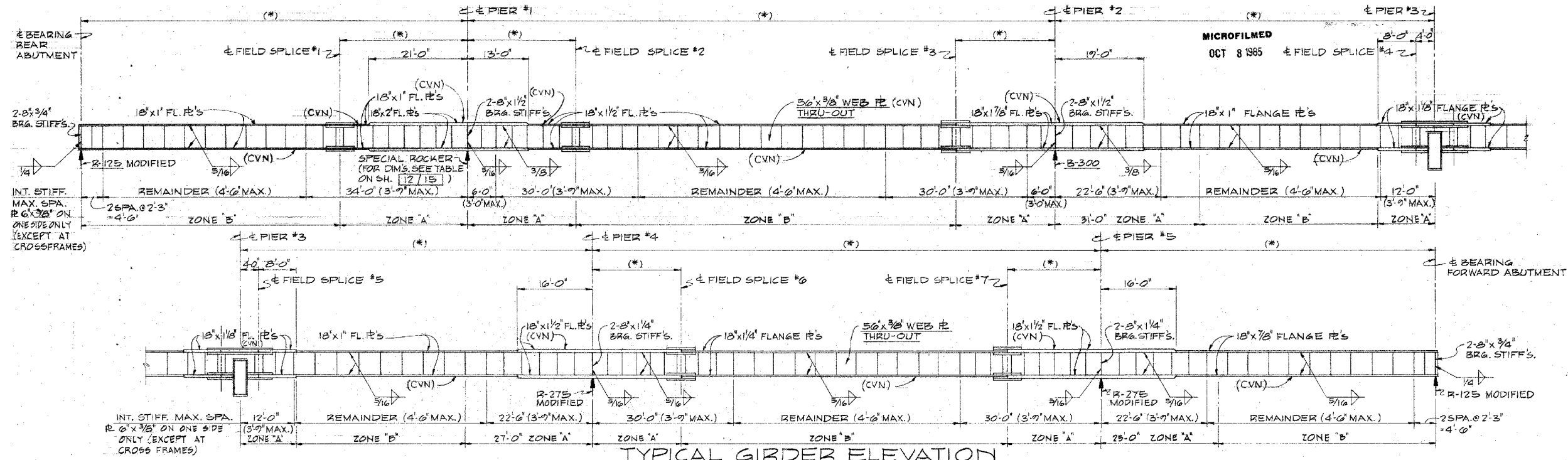
1 / 4

52
71



TRANSVERSE SECTION

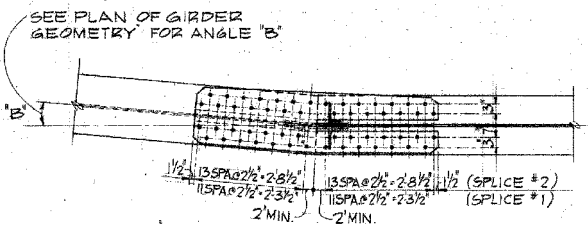
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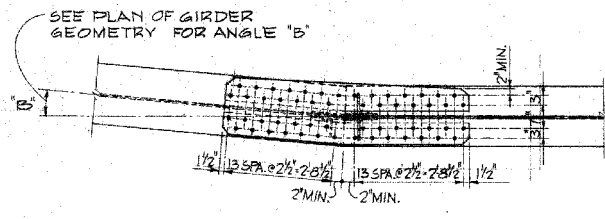
TYPICAL GIRDER ELEVATION

NOTE: WELD ATTACHMENTS MAY BE MADE TO THE TOP FLANGE WITHIN THE LIMITS OF ZONE 'B' ONLY.

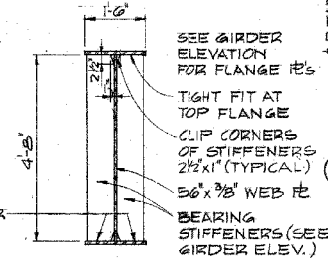
NOTE: (1*) SEE PLAN OF GIRDER GEOMETRY, SHEET 7/15 FOR ACTUAL LENGTH ALONG GIRDERS & LOCATIONS OF SPLICES



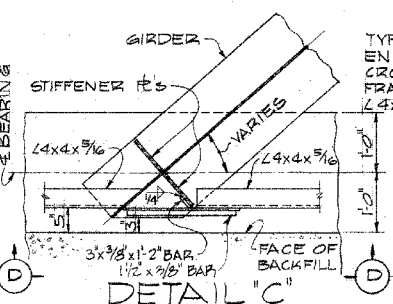
SECTION A-A



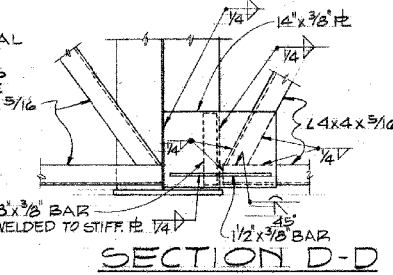
SECTION B-B



TYPICAL GIRDER SECTION AT ABUTMENTS & PIERS



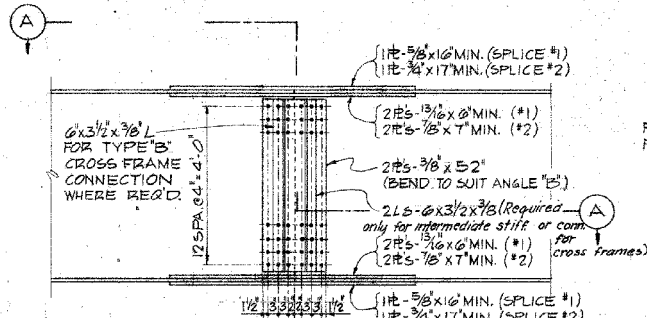
DETAIL C-C



SECTION D-D

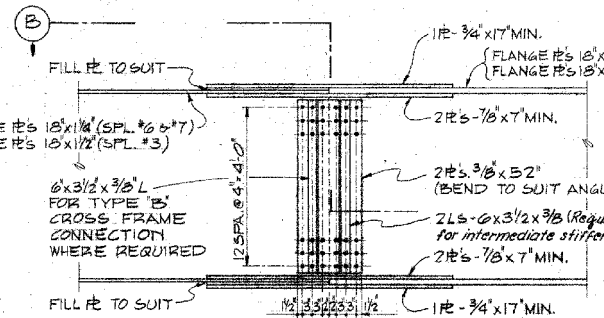
NOTES:

ROCKERS AND BOLSTERS SHALL BE R-125 AT REAR & FORWARD ABUTMENTS, B-300 AT PIER 2 AND R-275 AT PIERS 4 & 5. FOR DETAILS, SEE STANDARD DRAWING RB-1-55. EXCEPT THAT MASONRY PLATES FOR ROCKERS SHALL BE MODIFIED TO ACCOMMODATE TWO 1/4" ANCHOR RODS. FOR SIZE & DETAIL OF MODIFIED MASONRY PLATES AND DIMENSIONS OF SPECIAL ROCKERS AT PIER 1, SEE SH. 12/15. ALL ROCKERS & BOLSTERS SHALL BE PLACED NORMAL TO THE GIRDERS AT EACH SUBSTRUCTURE UNIT. FOR DETAILS OF END CROSSFRAME, END FINISH, SCUPPERS AND BEAM CUT-OFF AT BACKWALL, SEE STD. DWG. SD-1-69 FOR LOCATION OF DETAIL C' SEE SH. 7/15. WHERE A SHAPE OR PLATE IS DESIGNATED (CVN) THE MATERIAL SHALL MEET SPECIFIED MINIMUM NOTCH TOUGHNESS REQUIREMENTS.

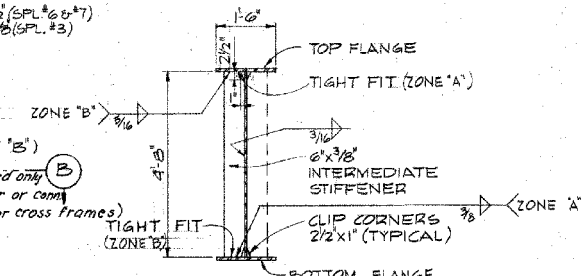


TYPICAL FIELD SPLICES #1 & #2
FASTENERS: 3/8" DIAMETER HIGH STRENGTH BOLTS (A325)

NOTE: REVERSE OUTSTANDING LEG OR MOVE STIFFENER ANGLE A DISTANCE EQUAL TO BOLT SPACING TO ADJUST TO INTERMEDIATE STIFFENER OR CROSS FRAME SPACING.
ALL SPICE PLATES, EXCEPT FOR FILLS, ARE CVN.

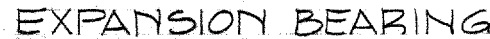


TYPICAL FIELD SPLICES #3, #6 & #7
FASTENERS: 3/8" DIAMETER HIGH STRENGTH BOLTS (A325)

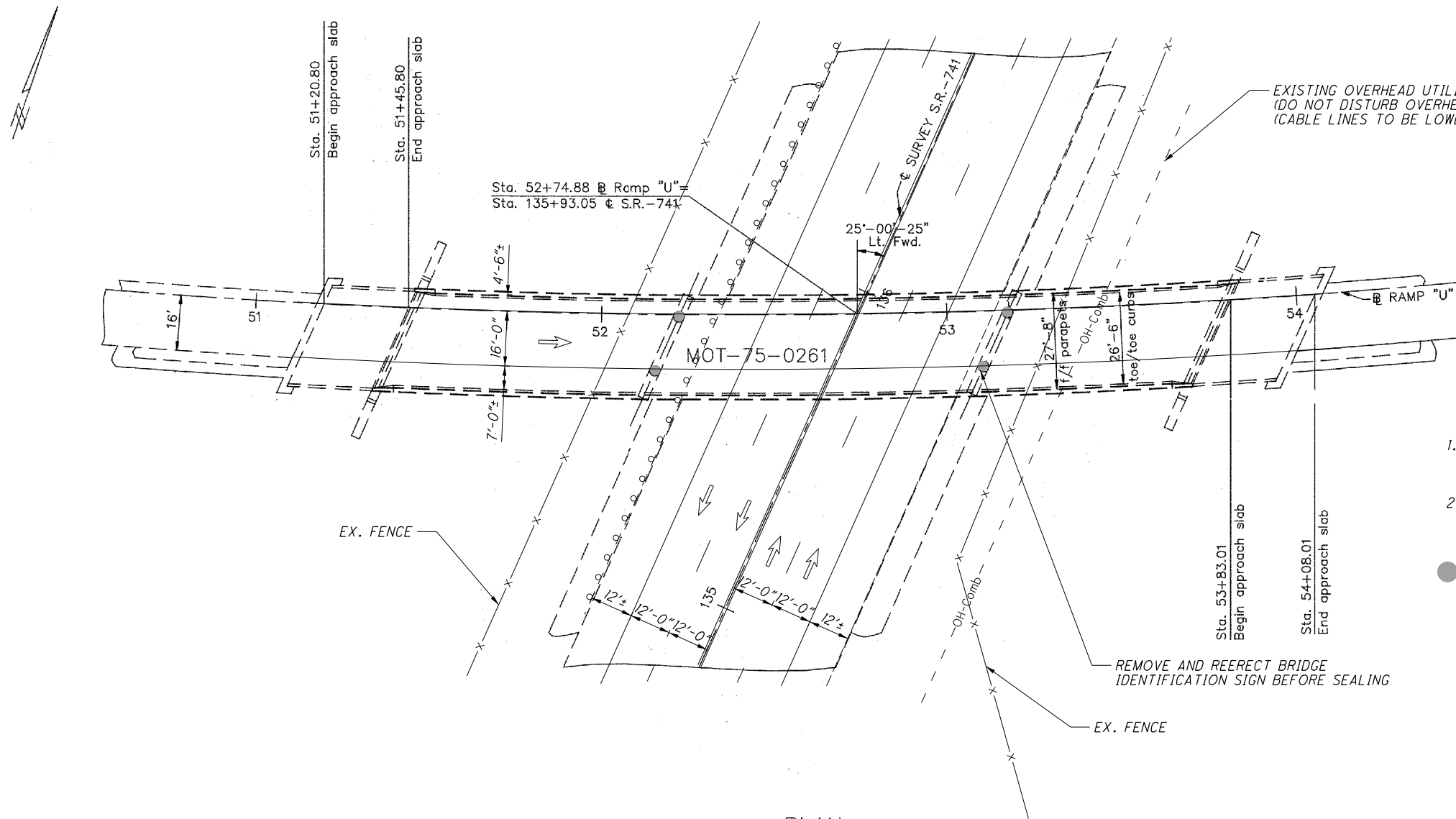


TYPICAL GIRDER SECTION AT INTERMEDIATE STIFFENERS

SINGLE STIFFENERS SHALL BE PLACED ON ALTERNATE SIDES OF THE WEB OF INTERIOR GIRDERS AND ON THE INSIDE OF THE WEB OF FASCIA GIRDERS. ADD STIFFENERS AS REQUIRED FOR THE ATTACHMENT OF CROSS FRAMES.



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NOTES

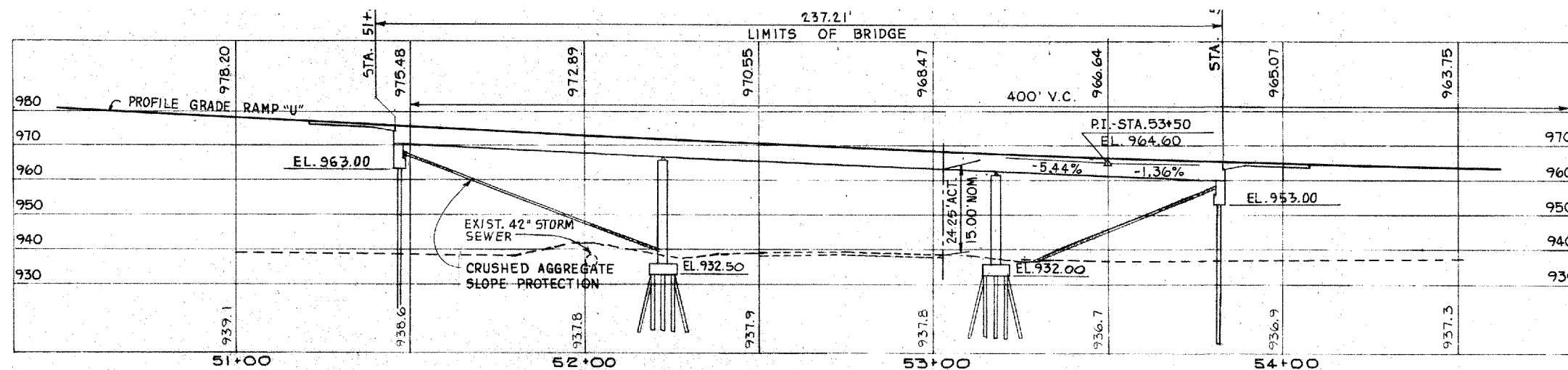
1. DETAILS SHOWN ARE TAKEN FROM EXISTING PLANS AND ARE FOR REFERENCE ONLY. ALL DIMENSIONS ARE CONSIDERED APPROXIMATE.
2. PIER COLUMNS ARE 3'± Ø. THE AVERAGE EXPOSED COLUMN HEIGHT IS 24'-0".

● - PIER COLUMNS TO BE SEALED PER ITEM 512 SEALING OF CONCRETE SURFACES (EPOXY-URETHANE)

REMOVE AND REERECT BRIDGE IDENTIFICATION SIGN BEFORE SEALING

EX. FENCE

PLAN



ELEVATION VIEW

PROPOSED WORK

TYPE: PAINT STRUCTURAL STEEL, BEAMS, CROSS FRAMES, END FRAMES, AND BEARINGS. SEAL PIER COLUMNS AS SHOWN WITH EPOXY URETHANE.

EXISTING STRUCTURE

TYPE: CONTINUOUS PLATE GIRDER WITH REINFORCED CONCRETE DECK AND SUBSTRUCTURE.

SPANS: 76.0'-95.0'-64.0' (ALONG @)

ROADWAY: 26'-6" TOE/TOE PARAPET

LOADING: HS20-44 CASE II & ALT. MILITARY LOADING

SKEW: 25°-00'-25" LT. FWD.

APPROACH SLABS: AS-1-72 (25' LONG)

ALIGNMENT: 2°-30' TO THE LEFT

CROWN: SUPERELEVATION 0.059'/FT

STRUCTURAL FILE NUMBER: 5710782

DATE BUILT: 1985

DISPOSITION: PAINT STRUCTURAL STEEL

DESIGN AGENCY
ODOT DISTRICT 7
PLANNING & ENGINEERING

REVIEWED
MRB
DATE
1/31/17
STRUCTURE FILE NUMBER
5710782

DRAWN
MEF
DESIGNED
MEF
CHECKED
CWW

MONTGOMERY COUNTY
STA. 51+00
STA. 54+50

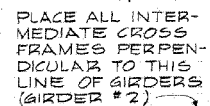
SITE PLAN

BRIDGE NO.: MOT-75-0261
IR-675 RAMP "U" OVER SR-741

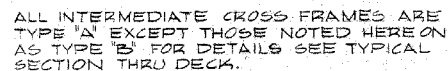
MOT-BP-FY18
PID No. 102303

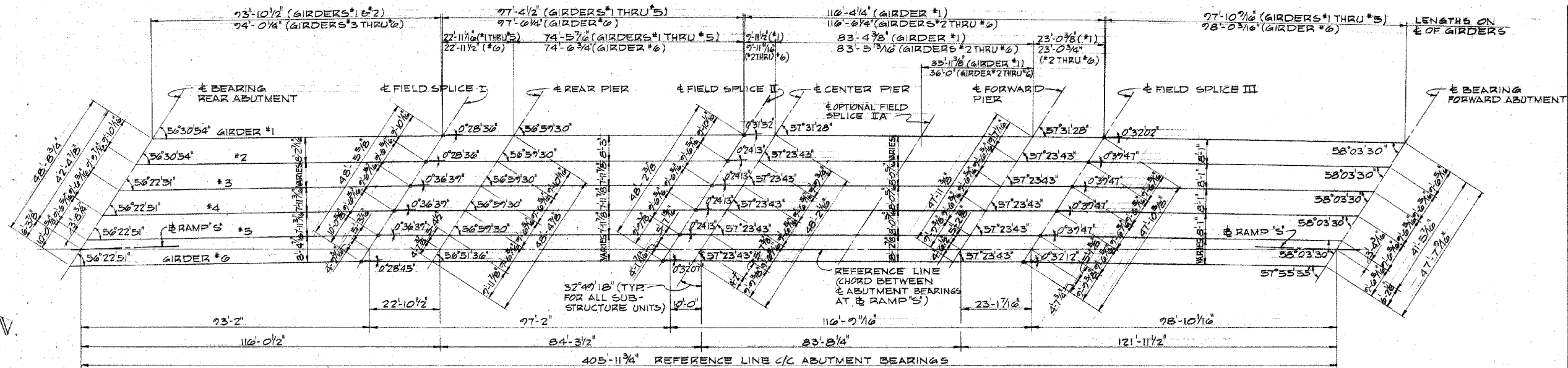
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71

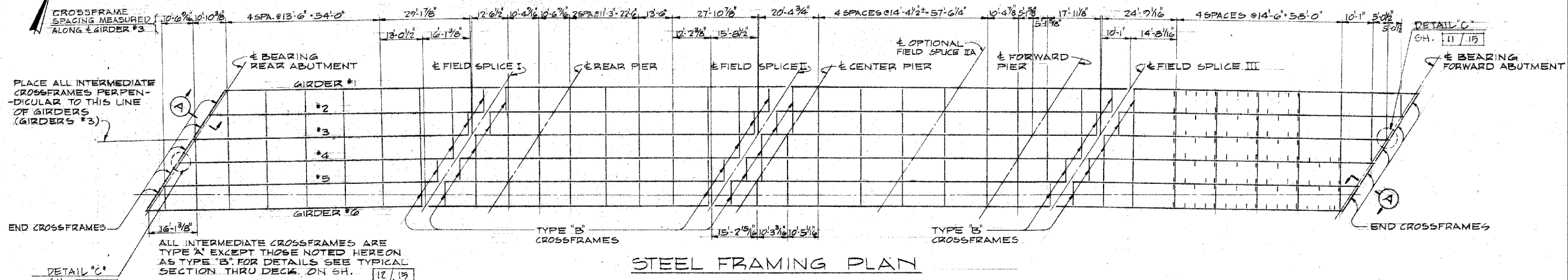


STEEL FRAMING PLAN

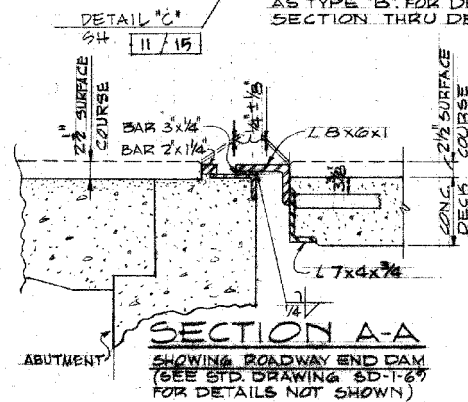




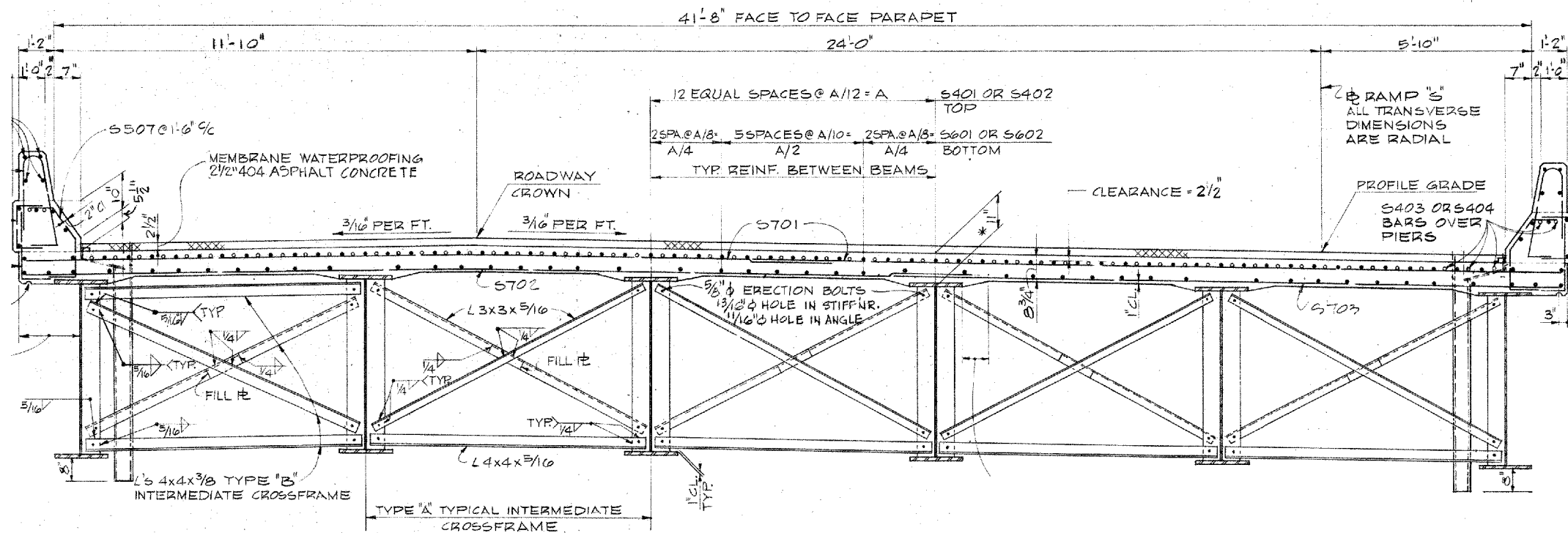
PLAN OF GIRDER GEOMETRY



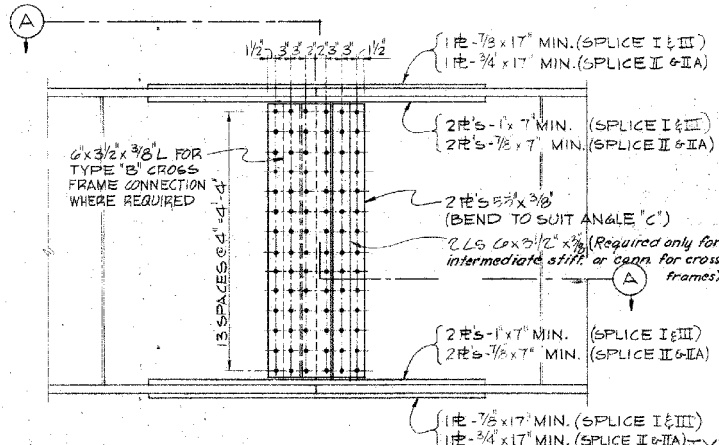
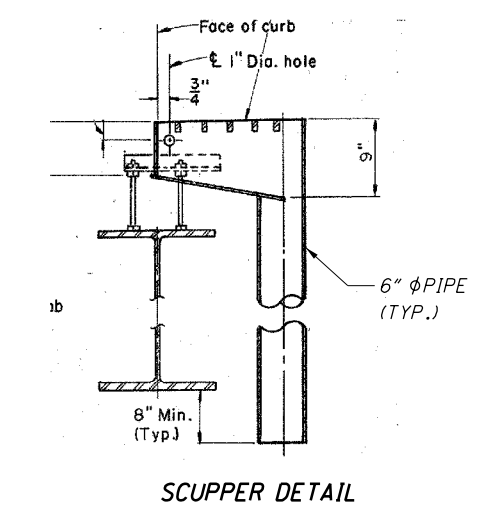
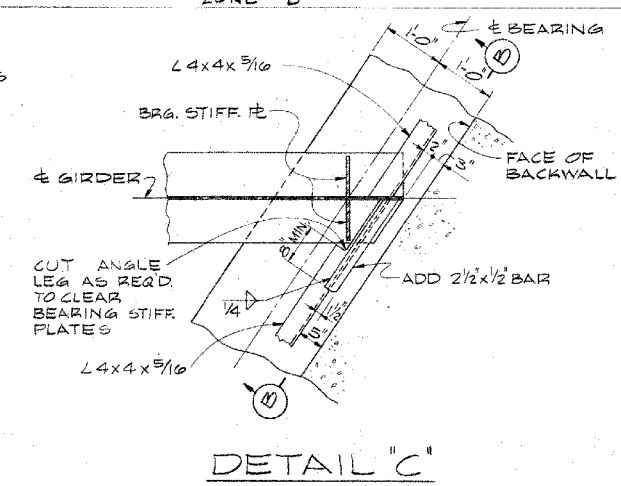
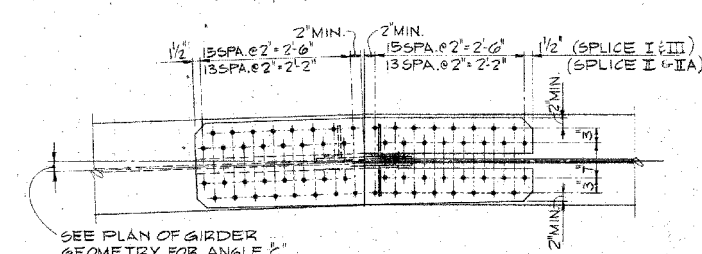
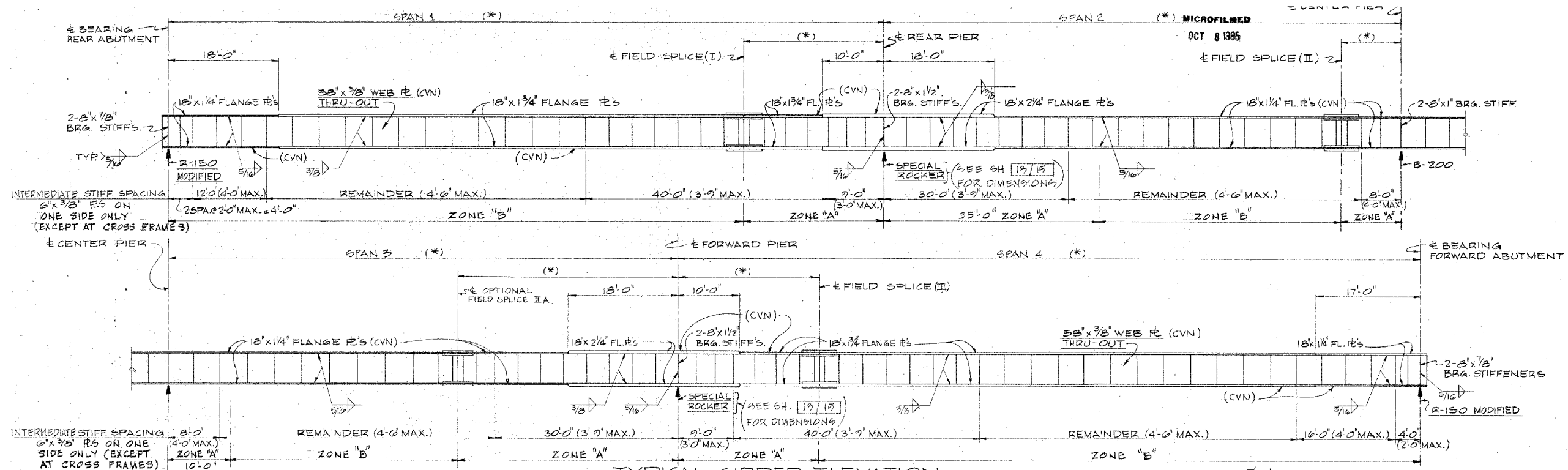
STEEL FRAMING PLAN



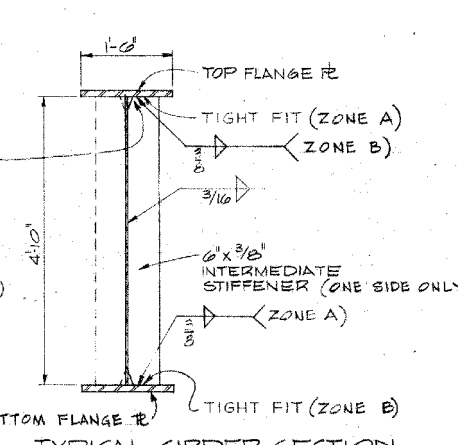
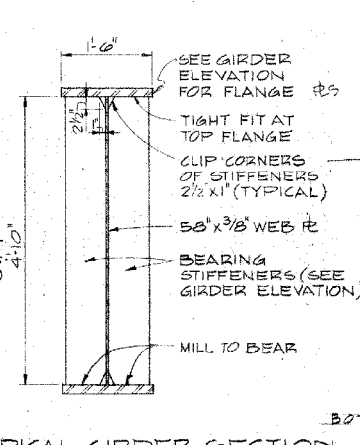
SECTION A-A
SHOWING ROADWAY END DAM
(SEE STD. DRAWING SD-T-67
FOR DETAILS NOT SHOWN)



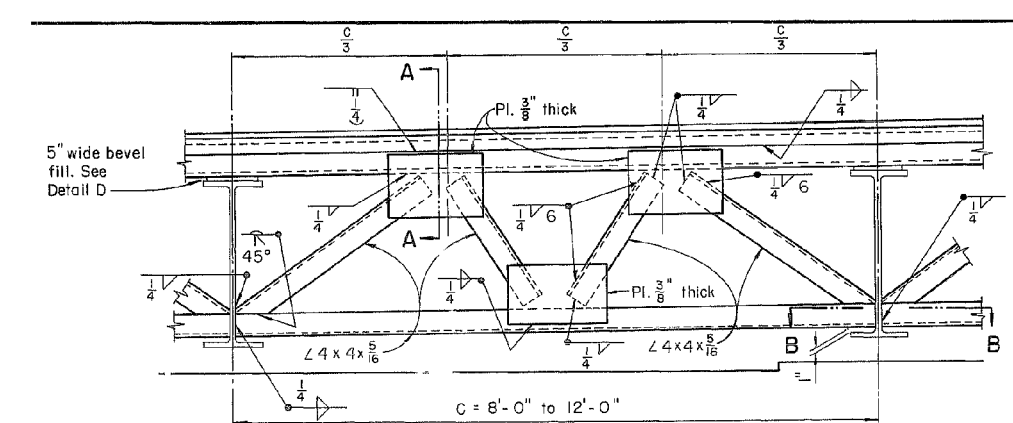
TRANSVERSE SECTION



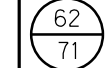
FASTENERS: 7/8" DIAMETER HIGH STRENGTH BOLTS (A325)
NOTE: REVERSE OUTSTANDING LEG OR MOVE STIFFENER ANGLE A DISTANCE EQUAL TO BOLT SPACING TO ADJUST TO INTERMEDIATE STIFFENER OR CROSSFRAME SPACING.

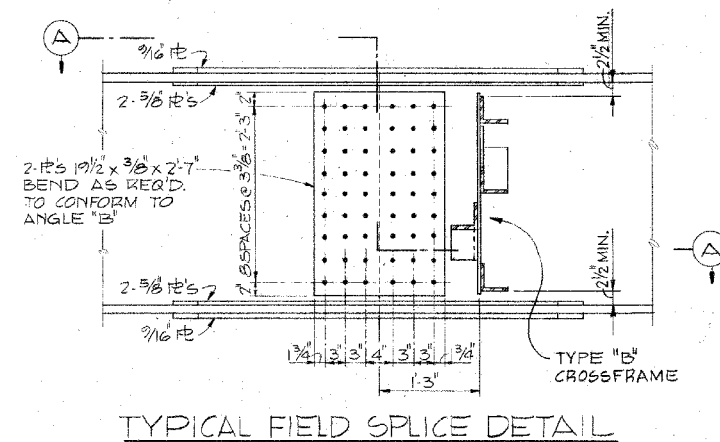
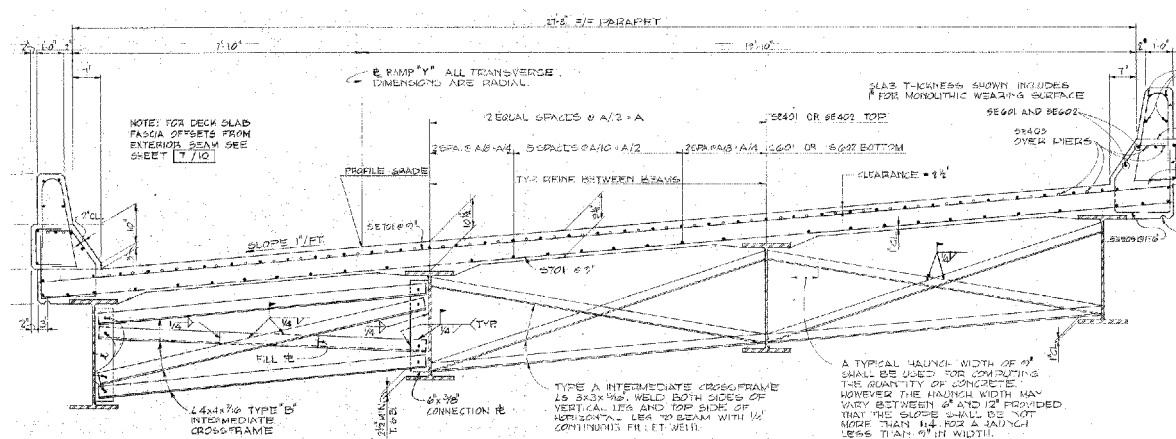
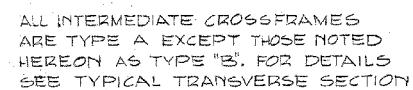
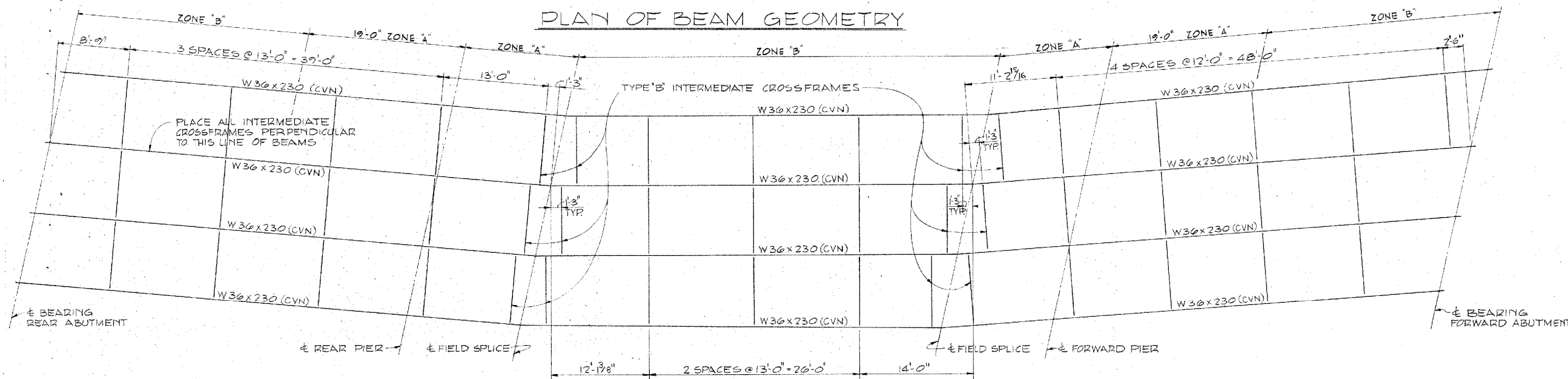
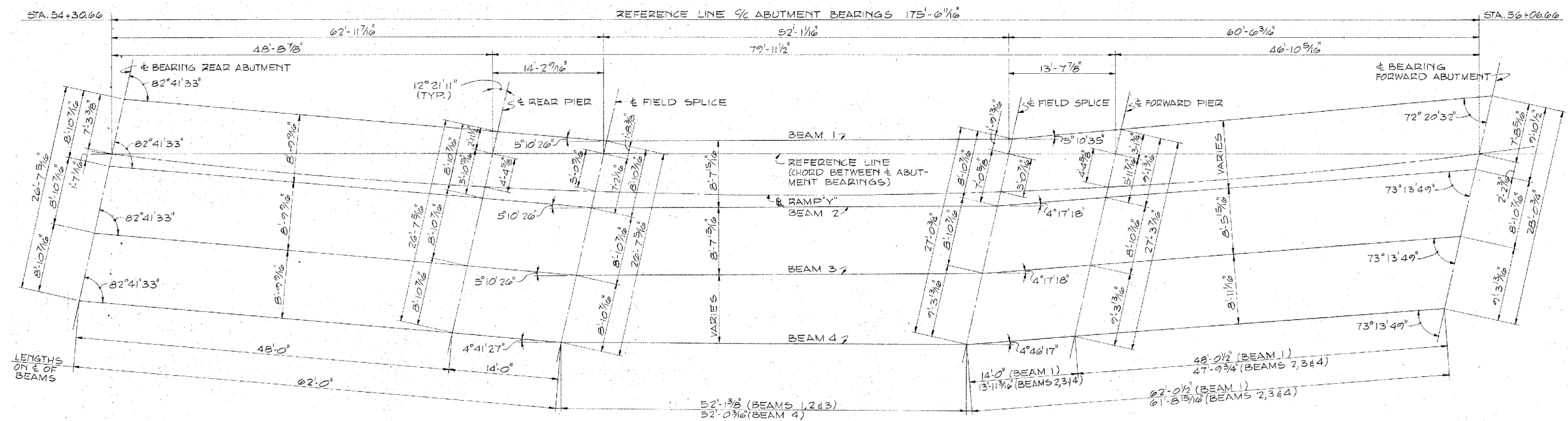


SINGLE STIFFENERS SHALL BE PLACED ON ALTERNATE SIDES OF THE WEB OF INTERIOR GIRDERS AND ON THE INSIDE OF THE WEB OF FASCIA GIRDERS. ADD STIFFENERS AS REQUIRED FOR THE ATTACHMENT OF CROSS FRAMES.



| | | | | |
|----------------------------|-----------------------|----------|---------|----------|
| DESIGN AGENCY | DATE | REVIEWED | DRAWN | DESIGNED |
| ODOT DISTRICT 7 | 1/31/17 | MRB | CWW | CWW |
| PLANNING & ENGINEERING | STRUCTURE FILE NUMBER | 5710707 | REVISED | CHECKED |
| SUPERSTRUCTURE DETAILS | | | | |
| BRIDGE NO.: MOT-675-0055L | | | | |
| IR-675 RAMP "S" OVER IR-75 | | | | |
| MOT-BP-FY18 | PID No. 102303 | | | |
| 3 | 3 | | | |
| 61 | 71 | | | |





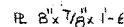
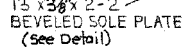


ALL DIMENSIONS CONSIDERED APPROXIMATE.

| | | | | | | | | | | | |
|--------|-------------------------------|------------|-----------|------------------------------------|-----------------|------------|---------------|------------|-----------|---------------|--------|
| 971.73 | | 970.74 | STA. 53+3 | 969.75 | 968.76 | 967.77 | 966.78 | 965.79 | STA. 55+5 | 964.80 | 963.81 |
| 970 | WESTBOUND I-675 PROFILE GRADE | | | | | | | | | - 1.98% SLOPE | 970 |
| 960 | | EL. 958.37 | | EL. 963.7 | | | | | EL. 959.4 | EL. 954.10 | 960 |
| 950 | | | | | | | | | | | 950 |
| 940 | | | | EXIST. | 42" STORM SEWER | 15.00' MIN | 26.72' ACTUAL | | | | 940 |
| 930 | | | | CRUSHED AGGREGATE SLOPE PROTECTION | EL. 934.79 | | | EL. 934.79 | | | 930 |
| 940.20 | 939.9 | 939.4 | 939.5 | 941.3 | 939.5 | 940.1 | 939.8 | 939.5 | | | |

TYPE: PAINT STRUCTURAL STEEL, BEAMS, CROSS FRAMES, END FRAMES, AND BEARINGS.

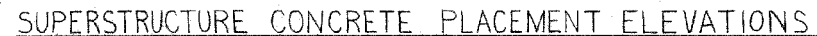
DISPOSITION: PAINT STRUCTURAL STEEL



AT CHANGE IN BEAM SIZES

FASTENERS : 1" DIAMETER HIGH STRENGTH BOLTS (A325)

ALL SPLICE PLATES, EXCEPT FOR FILLS, ARE CYN.



THE TOP OF CONCRETE ELEVATIONS SHOWN ABOVE SHALL GOVERN THE PLACING OF FORMS OR SCREEDS PRIOR TO PLACING THE DECK CONCRETE. ALLOWANCE HAS BEEN MADE FOR THE DEFLECTION DUE TO THE WEIGHT OF CONCRETE.

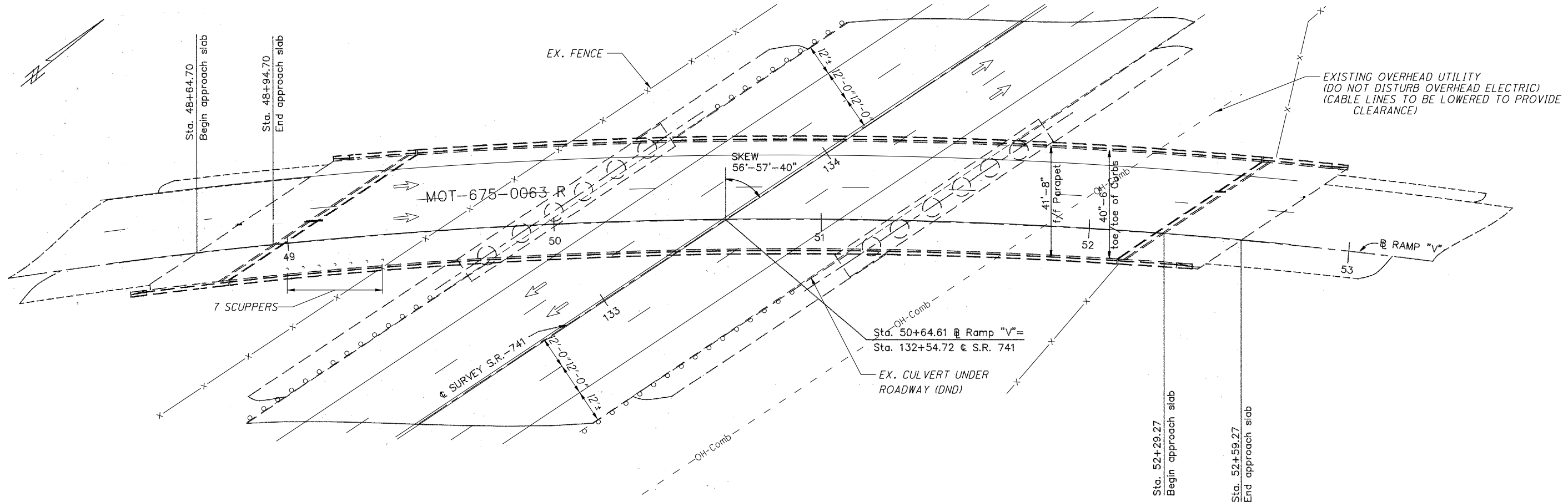
| | | | | | | | | | | | | | | |
|----|-------|------|-------|----|-------|-------|-------|------|----|-------|-------|-------|----|--|
| 0" | 1/16" | 1/8" | 1/16" | 0" | 1/16" | 1/8" | 3/16" | 1/8" | 0" | 0" | 1/16" | 1/16" | 0" | DEFLECTIONS DUE TO WEIGHT OF STEEL |
| 0" | 3/8" | 3/8" | 3/16" | 0" | 3/8" | 7/16" | 3/4" | 1/2" | 0" | 1/16" | 3/16" | 3/16" | 0" | DEFLECTIONS DUE TO REMAINING DEAD LOAD |
| 0" | 7/16" | 1/2" | 1/4" | 0" | 7/16" | 9/16" | 5/16" | 5/8" | 0" | 1/16" | 1/4" | 1/4" | 0" | REQUIRED SHOP CAMBER |

BEAM DEFLECTION AND CAMBER

NOTE: REQUIRED SHOP CAMBER MEASURED TO A CHORD BETWEEN ADJACENT BEARING POINTS.



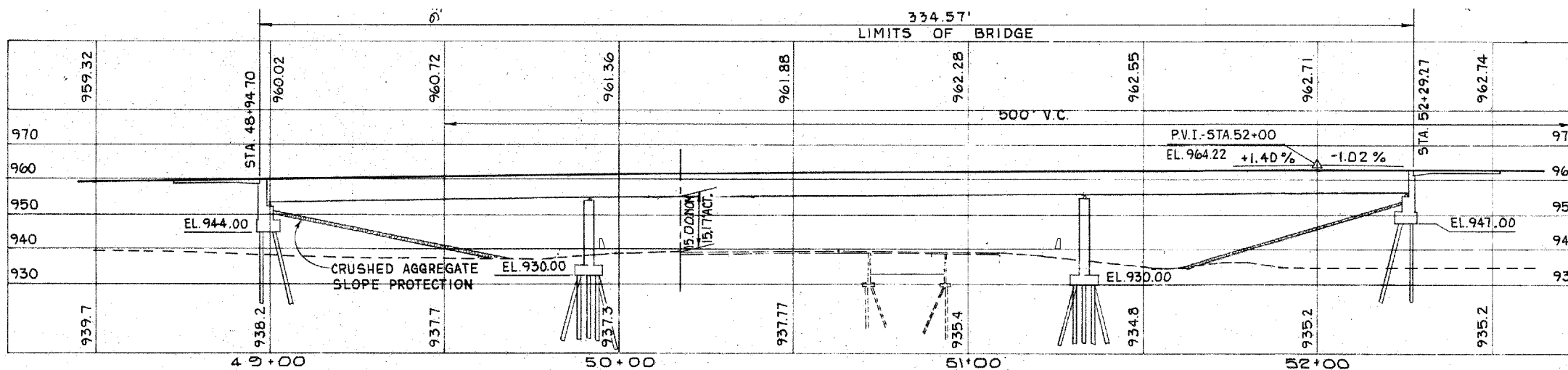
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NOTES

DETAILS SHOWN ARE TAKEN FROM EXISTING PLANS AND ARE FOR REFERENCE ONLY.

ALL DIMENSIONS CONSIDERED APPROXIMATE.



PROPOSED WORK

TYPE: PAINT STRUCTURAL STEEL, BEAMS, CROSS FRAMES, END FRAMES, SCUPPERS AND BEARINGS.

EXISTING STRUCTURE

TYPE: CONTINUOUS PLATE GIRDER WITH REINFORCED CONCRETE DECK AND SUBSTRUCTURE.

SPANS: 92.0' - 141.5' - 92.0' ALONG @
ROADWAY: 40'-6" TOE/TOE PARAPET
LOADING: HS20-44 CASE II & ALT. MILITARY LOADING
SKEW: 56°-57'-40" LT. FWD.

APPROACH SLABS: AS-1-72 (25' LONG)
ALIGNMENT: 3°-00' CURVE TO THE RIGHT
CROWN: SUPERELEVATION 0.071'/FT
STRUCTURAL FILE NUMBER: 5710804

DATE BUILT: 1983

DISPOSITION: PAINT STRUCTURAL STEEL

MOT-BP-FY18
PID No. 102303

SITE PLAN
BRIDGE NO.: MOT-675-0063 R
IR-675 RAMP "V" OVER SR-741

MONTGOMERY COUNTY
STA. 48+50
STA. 53+00

DESIGNED
MEF
CHECKED
CWW

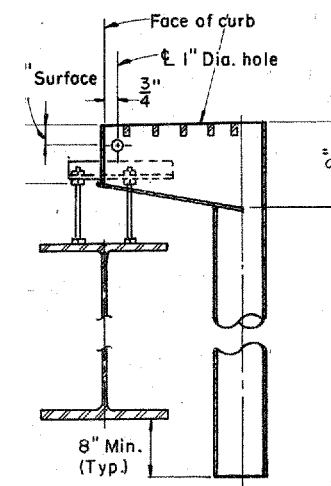
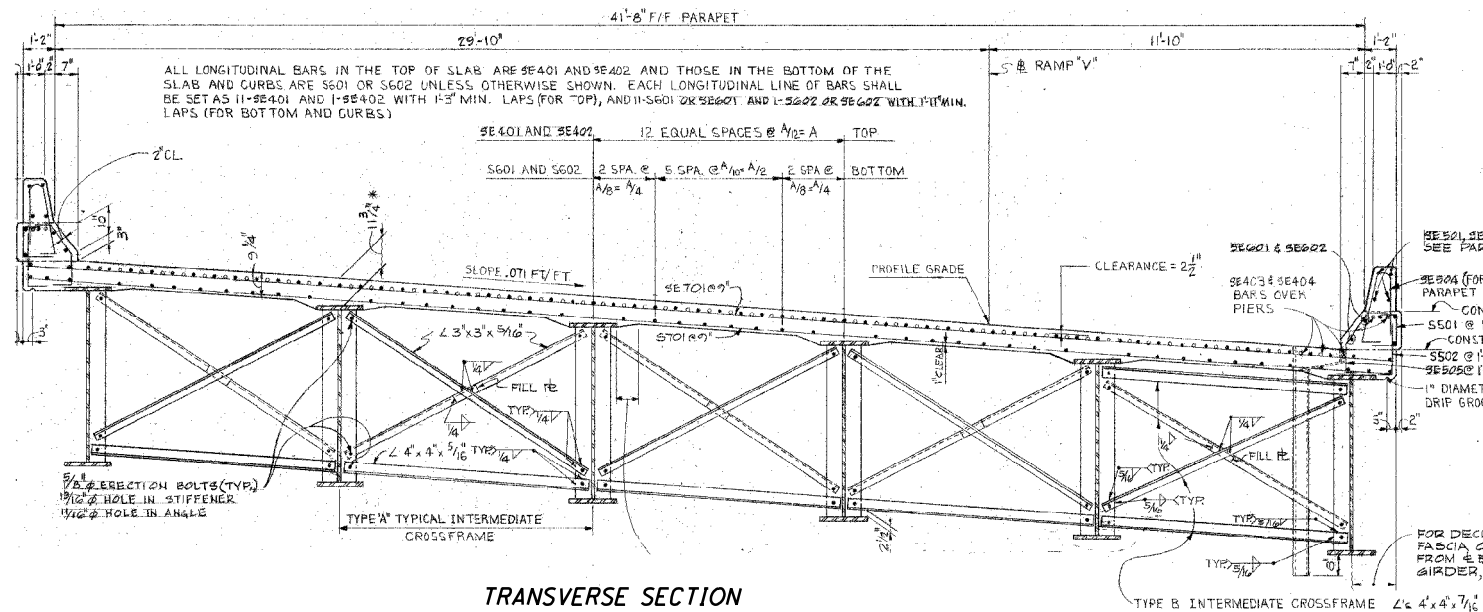
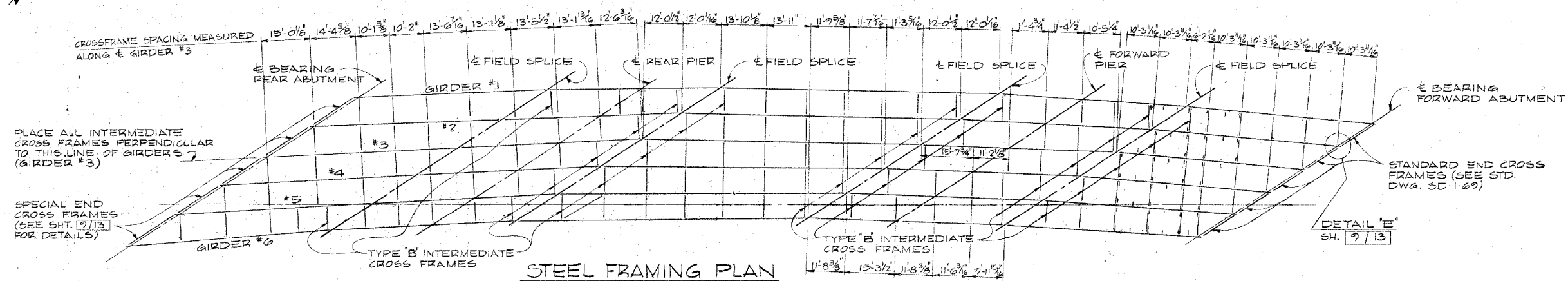
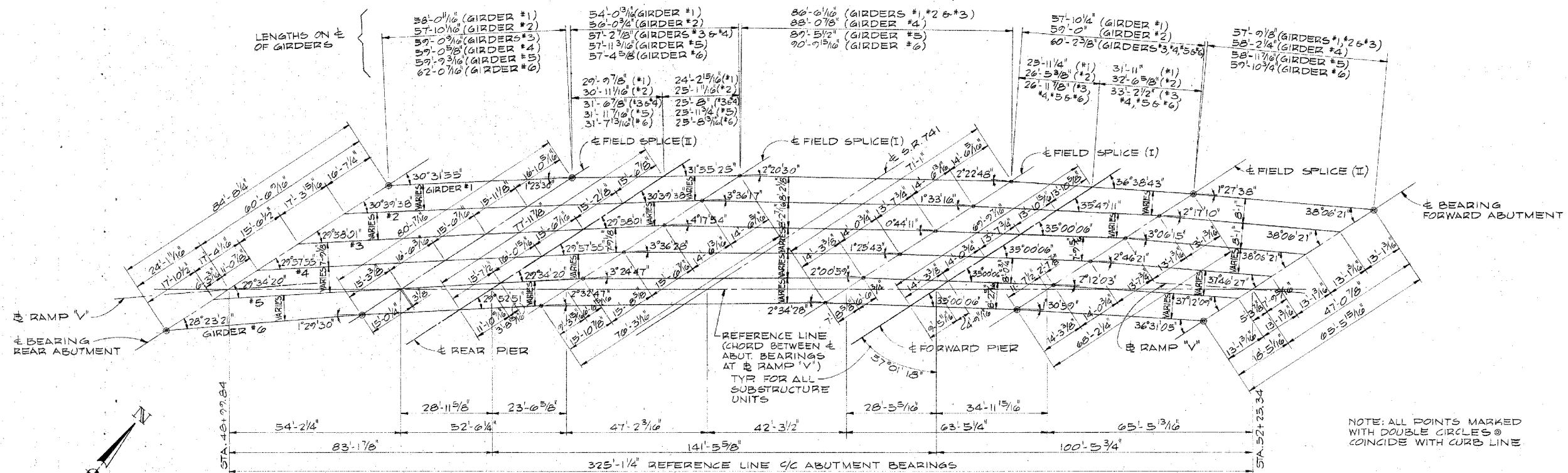
DRAWN
MEF
REVISED

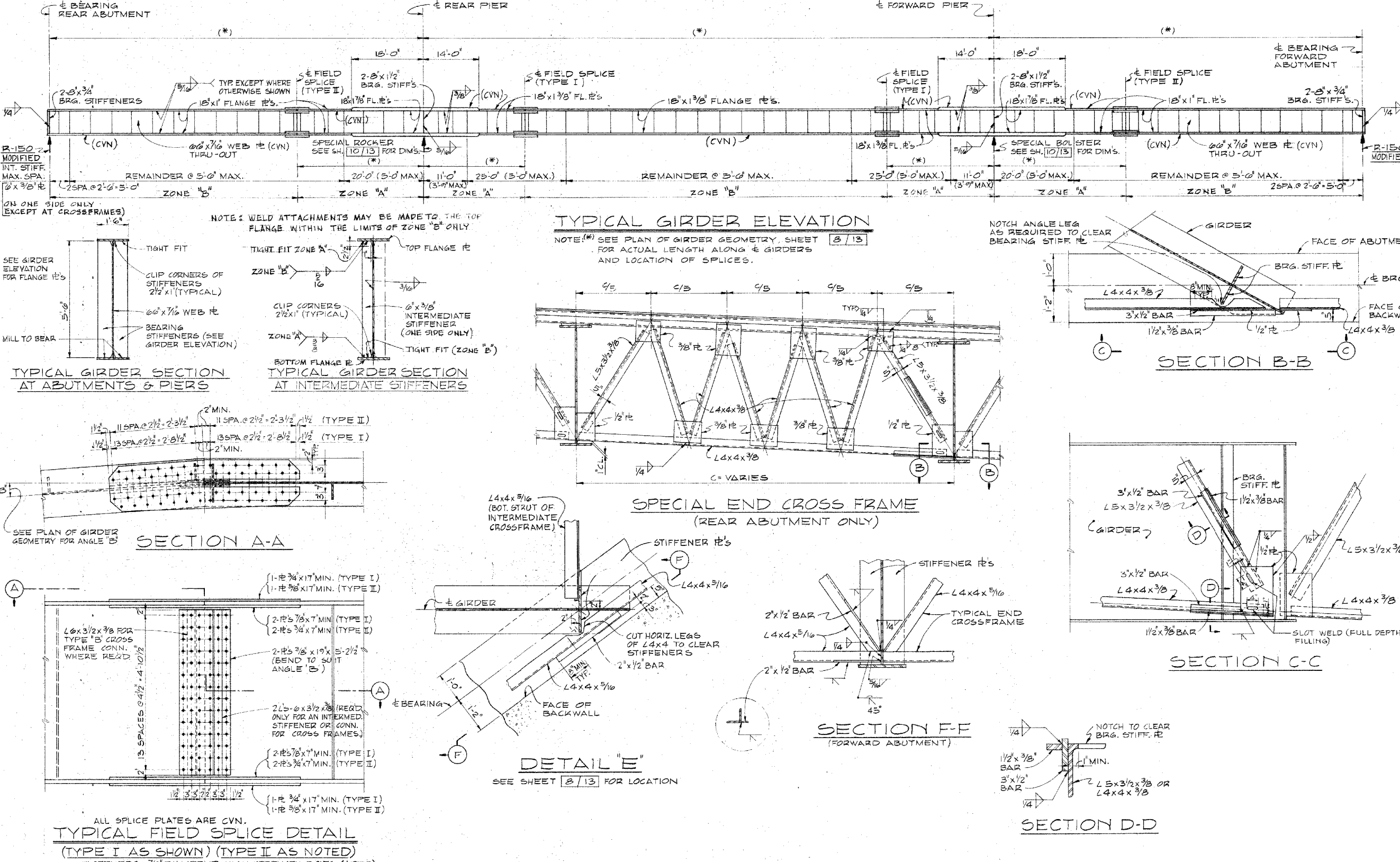
REVIEWED
MRB
STRUCTURE FILE NUMBER
5710804

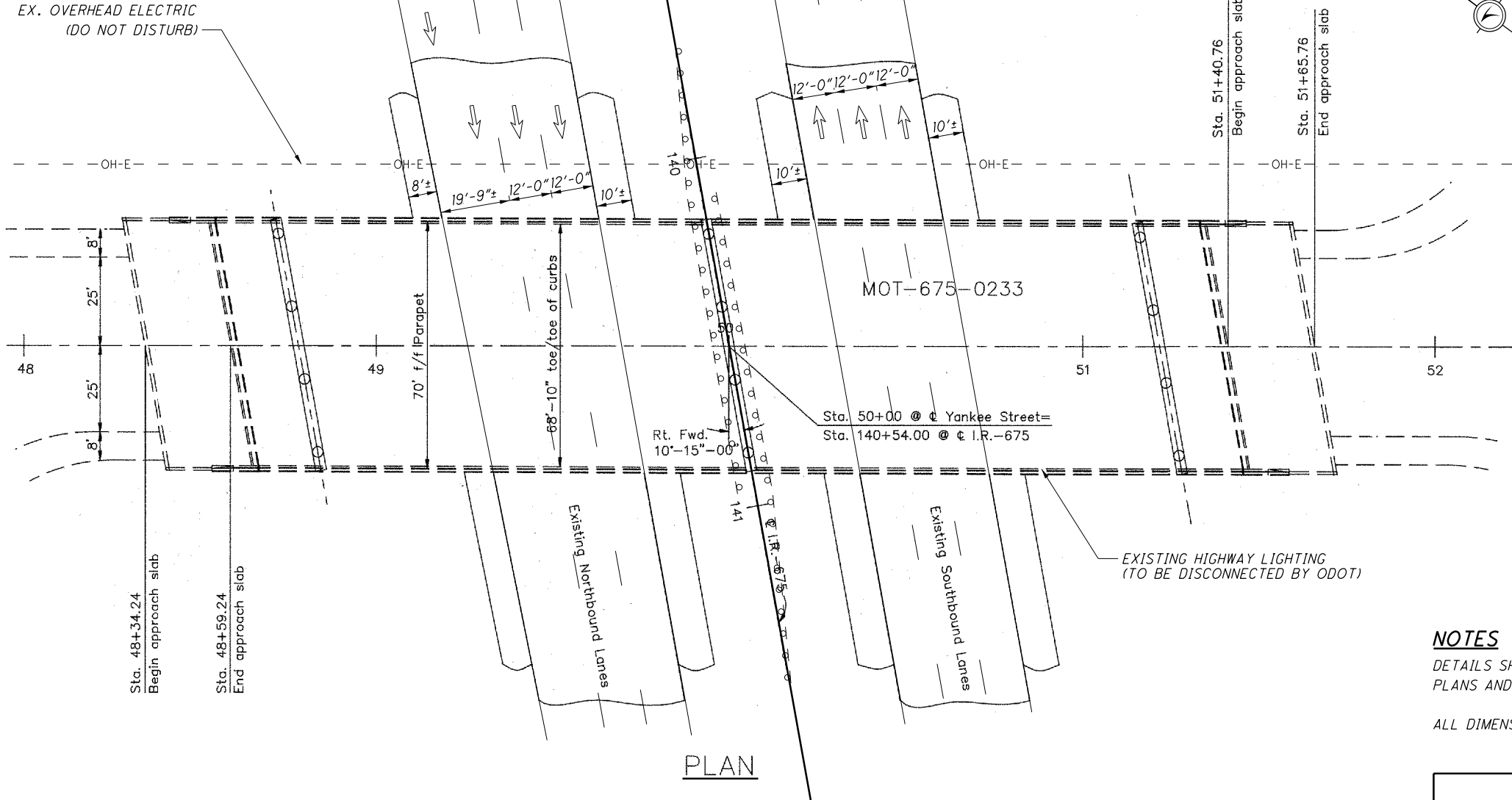
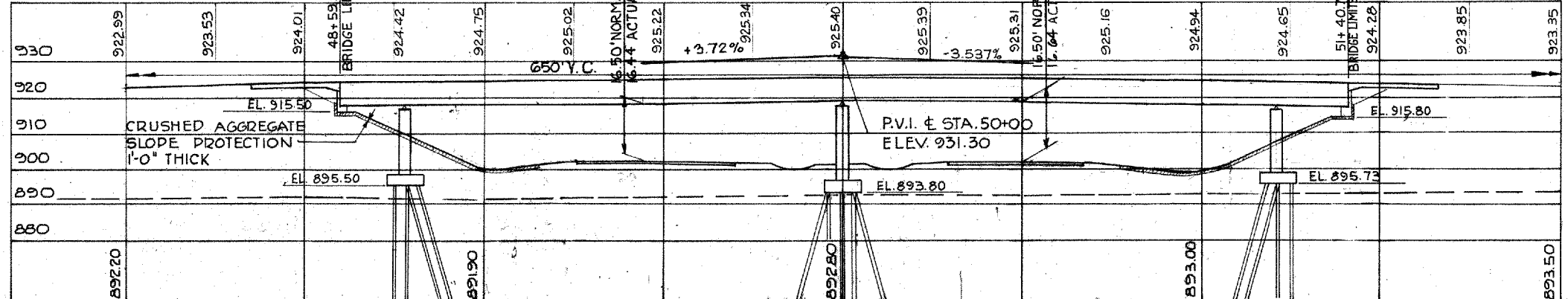
DESIGN AGENCY
ODOT DISTRICT 7
PLANNING & ENGINEERING

1/3

66
71







NOTES

DETAILS SHOWN ARE TAKEN FROM EXISTING
PLANS AND ARE FOR REFERENCE ONLY.

ALL DIMENSIONS CONSIDERED APPROXIMATE.

PROPOSED WORK

TYPE: PAINT STRUCTURAL STEEL, BEAMS, CROSS FRAMES,
END FRAMES, SCUPPERS AND BEARINGS.

EXISTING STRUCTURE

TYPE: CONTINUOUS STEEL GIRDER WITH REINFORCED CONCRETE DECK AND SUBSTRUCTURE.

SPANS: 18.0' - 122.0' - 122.0' - 18.0' c/c BRGS.

ROADWAY: 68'-10" TOE/TOE PARAPETS

LOADING: HS20-44 CASE II & ALT. MILITARY LOADING

SKEW: 10°-15'-00" RT. FWD.

APPROACH SLABS: AS-1-72 (25' LONG)

ALIGNMENT: TANGENT

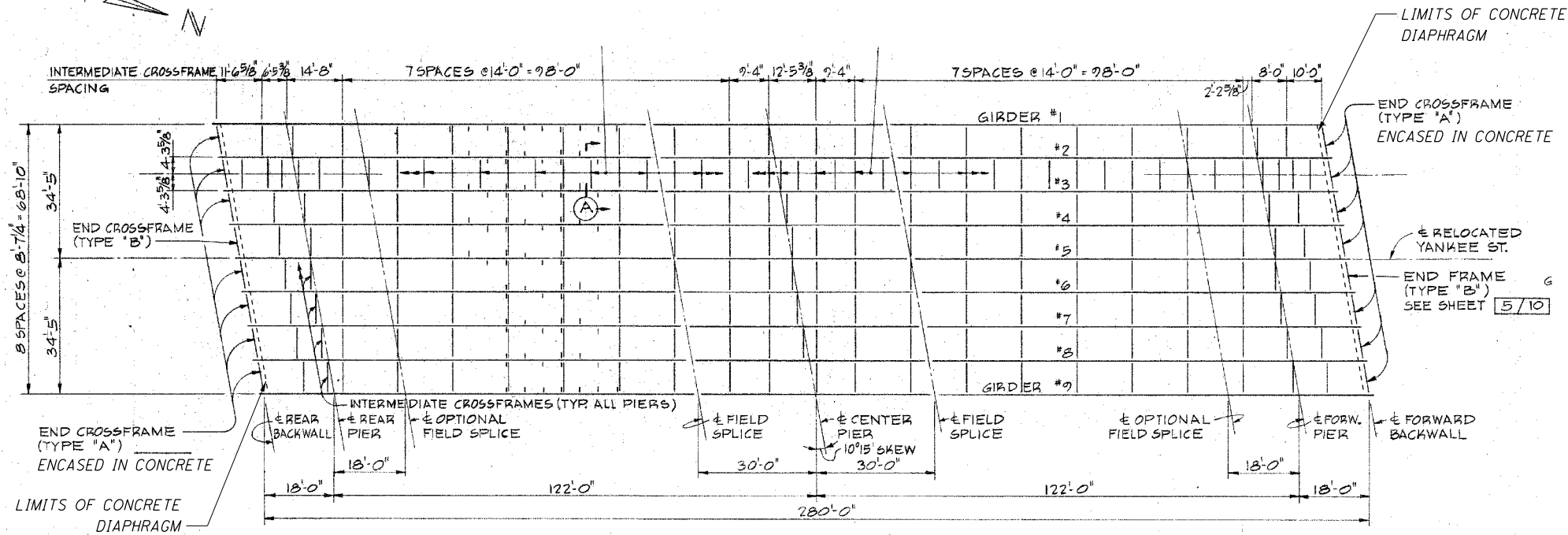
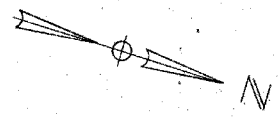
CROWN: $\frac{3}{16}$ "/FT

STRUCTURAL FILE NUMBER: 5711126

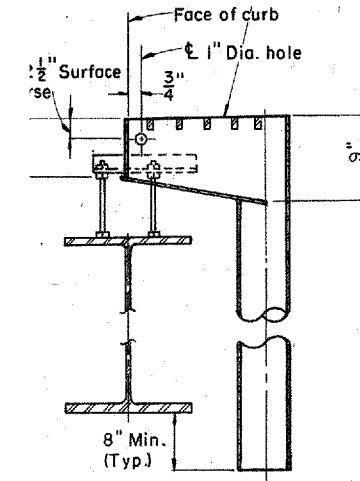
DATE BUILT: 1983

DISPOSITION: PAINT STRUCTURAL STEEL

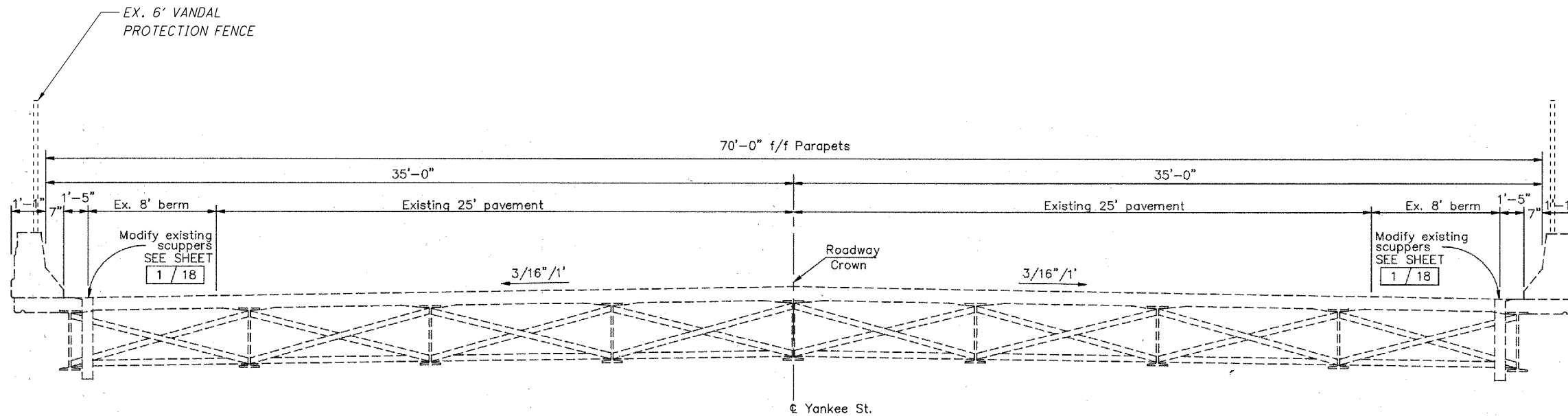
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STEEL FRAMING PLAN



SCUPPER DETAIL



TRANSVERSE SECTION

